

City of Beaumont
Subdivision and Development Appeal Board Agenda
SDAB Hearing 26-01 to Follow SDAB Hearing 25-09

February 23, 2026
Council Chambers, Beaumont Administration Office

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Notice of Hearing

File: 0111-S01-SDAB-26-01

APPELLANT
Kevin Panter

APPLICANT
Millbree Architect Art and Design c/o Sabrina
Keichinger

RESPONDENT
Craig Thomas, Development Authority
5600 49 Street, Beaumont, AB T4X 1A1
development@beaumont.ab.ca

February 2, 2026

Appeal # SDAB-26-01

Proposed Development: Two 2-storey Commercial (CRU) Buildings – East Building includes Daycare on upper floor with retail - service general and restaurant/café uses below.

Legal Description: Plan 912 1656, Block 103, Lot 1

Municipal Address: 9 Colonial Way, Beaumont, AB

Land Use District: Integrated Neighbourhood

Permit Application No: 2025-077

To Whom It May Concern:

This correspondence serves as your notice that the Subdivision and Development Appeal Board (SDAB) will hold a hearing in response to an appeal of a decision of the Development Authority to Approve Proposed Development with Conditions - Two 2-storey Commercial (CRU) Buildings – East Building includes Daycare on upper floor with retail - service general and restaurant/café uses below. Legal Description - Plan 912 1656, Block 103, Lot 1, located at 9 Coloniale Way, Beaumont, AB.

The SDAB will hold the hearing as follows:

DATE: Monday, February 23, 2026
TIME: 5:00 pm
LOCATION: City Hall Council Chambers, 5600 – 49 Street, Beaumont

You or any person acting on your behalf may present verbal, visual, or written submissions to the SDAB at the hearing.

If you wish to submit visual or written material to the SDAB, please email your submissions to the clerk at legislative@beaumont.ab.ca no later than **end of day at 4:30 pm on February 17, 2026**. All materials submitted will be included in the hearing package and will be distributed to the hearing participants in advance of the hearing.



BEAUMONT
**Subdivision
and Development**
APPEAL BOARD

The hearing materials will also be made available for public inspection by appointment during regular business hours (8:30 am to noon and 1pm to 4:30 pm, Monday to Friday) at the City Administration Building (5600 49 Street, Beaumont) **beginning on February 18, 2026.** Please contact the clerk at legislative@beaumont.ab.ca to arrange an appointment.

Important Information:

1. Any visual or written material received by the clerk in advance of the hearing will form part of the public record and will be made available for public inspection pursuant to section 686(4) of the *Municipal Government Act*, RSA 2000, c M-26;
2. While the clerk of the SDAB will accept visual or written material in advance of the hearing, the decision on what materials will be considered by the SDAB remains with the SDAB; and
3. Depending on the complexity and volume of the materials submitted, there may be requests for adjournments which the SDAB would consider on a case-by-case basis.

If you have questions, please contact the clerk at legislative@beaumont.ab.ca or call the City Hall main line at 780-929-8782.

Respectfully,

Joanne Dargis,
Clerk, Subdivision and Development Appeal Board

SUBDIVISION & DEVELOPMENT APPEAL BOARD HEARING
SDAB-25-08
October 1, 2025

DEVELOPMENT AUTHORITY'S REPORT

| | |
|----------------------------------|---|
| Permit Number: | 2025-077 |
| Decision: | Approved with conditions. |
| Subject Property: | 9 Coloniale Way Plan 912 1656, Block 103, Lot 1 |
| Land Use District: | Integrated Neighbourhood (IN) District |
| Subject Site Description: | Southeast corner of 50 th Street and Coloniale Way |
| Proposed Development: | Two 2-Storey Commercial (CRU) Buildings – Daycare, Restaurant/Café, and Retail & Service General Uses |

1. Context

The subject property is legally described as Plan 912 1656, Block 103, Lot 1, and is districted Integrated Neighbourhood (IN) under the City of Beaumont Land Use Bylaw (LUB).

On June 25, 2025, the application was received for 2-Two-Storey Commercial (CRU) Buildings – Proposed Uses: East Building proposed daycare use on the upper floor with retail and general service + restaurant/café uses below. West building proposes office use on upper floor with retail and service general + restaurant café uses below.

On January 5, 2026, the Development Authority authorized, subject to twelve conditions, the construction of two (2) two-storey buildings, with a total floor area of 4,993 square metres (East Building: 2,152 square metres; West Building: 2,841 square metres), a total building footprint of 2,727 square metres, and associated site improvements, in accordance with the stamped approved drawings and the provisions of the City of Beaumont Land Use Bylaw 944-19. The Development Permit authorized the following uses:

Education, a Discretionary Use in accordance with Section 3.4.2 of Land Use Bylaw 944-19. This use includes:

- 750 square metres of floor area located on the second floor of the east building (E201) as shown on Drawing number DP2.1; and
- 420 square metres of outdoor play area, comprised of:
 - 315 square metres located on the roof above the first floor as shown on Drawing number DP2.1; and
 - 105 square metres located at grade between the east façade and the east property line as shown on Drawing number 2.0.

Restaurant/Café, a Discretionary Use in accordance with Section 3.4.2 of Land Use Bylaw 944-19. This use is authorized within units W101, W109, W110, E104, E105, E108, and E109 as shown on Drawing numbers DP2.0, and DP2.3.

Retail & Service – General, a Permitted Use in accordance with Section 3.4.2 of Land Use Bylaw 944-19. This use is authorized within the remaining floor area as shown on Drawing Numbers DP2.0, DP2.3, and DP2.4.

Following the Notice of Decision, a 4' X 6' sign was erected on the subject site to serve as notice to nearby residents in accordance with Section 5.19.3 of the Land Use Bylaw.

As a result of the notification, one appeal was filed.

2. Policy Framework

The relevant policy and regulatory framework includes the following:

Municipal Development Plan (MDP):

- Section 2.6 Generalized Land Use Concept. This section states that Map 9 – Land Use Concept, details the land use policy areas to guide the long-term growth and development of Beaumont. Ten generalized character areas have been identified based on current development and the desired future development as established through the Our Complete Community vision and key goals. The land use concept has been designed to align with the growth directives in the Edmonton Metropolitan Region Growth Plan to appropriately intensify Beaumont's built-up and planned areas, in addition to ensuring that Beaumont's greenfield areas achieve a density of 35 dwelling units per net residential hectare along with supporting transit, employment, urban agriculture, and affordable housing opportunities.
- Map 9 of the Municipal Development Plan identifies this parcel as "Commercial".
- Section 2.6 articulates the character areas including commercial where it states: Commercial uses for areas identified on Map 9 – Land Use Concept include concentrations of retail, office, and entertainment uses, including large-format stores that attract a community-wide customer base. The policy area also includes a broad range of commercial uses that rely on highway and major corridor access and exposure, such as auto services, sales and gas stations, hotels, restaurants, and other hospitality services. These areas are designed to take advantage of key transportation routes and integrate highway commercial opportunities.
- Section 4.13.1: The development of Commercial uses will be encouraged in the locations indicated on Map 9 - Land Use Concept. Uses include retail, office, hospitality, and entertainment uses, including large-format stores that attract a community-wide customer base along key transportation routes to provide access and exposure. Multi-unit residential developments may also be integrated into the upper storeys of commercial buildings.
- Section 4.13.2: Commercial uses shall be situated along major roadways (major collector and arterials) to ensure convenient access, freight movement, and to minimize land use conflicts.
 - Map 7 of the MDP identifies 50th Street as an Arterial Road and Coloniale Way as a Collector (Major).

- In accordance with the City's Transportation Master Plan, 50th Street is classified as an Arterial Road, Coloniale Way and Rue Bouchard are Collector Roads.
- Section 4.13.4: Commercial uses shall be oriented to align with public roadways to create a pedestrian friendly streetscape and engaging public realm.
- Section 4.13.5: Multi-storey commercial buildings that efficiently use land and infrastructure are encouraged.
- Section 4.13.6: New commercial development proposals shall be subject to: a) an approved Area Structure Plan; b) An evaluation of servicing capacity and the provision of required infrastructure to be provided at the developer's expense; c) traffic assessments for appropriate circulation and access; and d) all costs of development shall be borne by the developer.
- Section 5.1.6: The development of storefront commercial businesses, including: diverse shopping opportunities, professional services, personal services, national chains, niche market businesses, and neighbourhood-based businesses will be encouraged.
- Section 5.1.18: Neighbourhood commercial uses will be encouraged to locate in Future Residential Policy areas in centralized locations adjacent to multi-modal transportation corridors.

Coloniale Estates Outline Plan:

The Coloniale Estates Outline Plan was approved by Council on February 12, 2008. The plan identifies the subject parcel as Commercial.

Land Use Bylaw (LUB):

- Section 3.4.2. Identifies Uses that are classified as either Permitted or Discretionary within IN – Integrated Neighbourhood Commercial District. Development Permit 2025-077 authorized Retail & Service General which is listed as a Permitted Use; as well as Education (Daycare) and Restaurant/Café, which are both listed as Discretionary Uses.
- The Land Use Bylaw provides Development Standards that govern the manner in which developments are to be affected. This includes yard space (setbacks from property lines), height, parking requirements, landscaping, among other things.

3. Relevant Background

June 26, 2025: Development Permit Application was submitted.

July 14, 2025: Deemed Incomplete Notice from the Development Authority was sent to the applicant.

July 22, 2025: Deemed Complete Notice from the Development Authority was sent to the application.

November 7, 2025: Pre-Decision Signs erected onsite. In accordance with Section 5.16 (Application Notification Requirements), prior to approving an application for a development which is a discretionary use, ... the Development Authority may require the applicant to post a notice on the property in a location and format determined by the Municipality describing the proposed development and advising any interested parties where further information regarding the application may be obtained. Such notice shall be posted for a minimum of 10 calendar days prior to the issuance of a notice of decision. The sign solicited feedback from residents by providing an email address or by scanning a QR Code. The sign The Sign remained onsite until December 5, 2025.

December 29, 2025: Engagement Report for 9 Coloniale Way Development was published on the City's website. This report "What We Heard & Did", provided a summary of the timeline and process, provided the themes of the engagement feedback, how the City responded, the next steps moving forward, and redacted submissions from residents.

January 2, 2026: Engagement Report for 9 Coloniale Way Development was emailed to residents who submitted feedback.

January 5, 2026: Development Permit was issued subject to twelve (12) conditions.

January 9, 2026: Three signs were posted on the site in accordance with Section 5.19 – Notice of Decision. This section states that when an application for a development is approved for a discretionary use, or for a permitted use in which a variance has been granted, the Development Authority shall provide notice of the decision that will include a description of the proposed development, state the decision, and advise of the right of appeal.

4. Regulation Review and Development Authority's Justification for Decision

The Development Authority approved a Development Permit authorizing a commercial Development comprising two buildings with a total gross floor area of 4,993 square metres, a total building footprint of 2,727 square metres, and associated site improvements.

The Development Permit authorizes:

- Education, a Discretionary Use;
- Restaurant/Café, a Discretionary Use; and
- Retail & Service – General, a Permitted Use.

The Development Authority interpreted that the Development complied with the Land Use Bylaw's development standards and therefore no variances were approved as part of this application. Accordingly, the Development Authority's evaluated whether the Discretionary Uses would be reasonably compatible with nearby properties.

Discretionary Use Evaluation and Compatibility

Under Section 3.4.2 of Land Use Bylaw 944-19, Education and Restaurant/Café uses are listed as Discretionary Uses within the applicable land use district. A discretionary use is a use that may be approved at the discretion of the Development Authority, provided that it is reasonably compatible with surrounding land uses and complies with the intent of the district and applicable statutory plans.

In exercising this discretion, the Development Authority considered:

- The nature, scale, and intensity of the proposed uses;
- The siting and design of the buildings and outdoor amenity areas;
- The location of the site within an established commercial policy area;
- The transportation context and access to arterial and collector roadways; and
- The absence of any requested variances or regulatory non-compliance.

The Development Authority determined that the proposed uses are consistent with the intended function of the site and are compatible

with existing surrounding development, particularly given the site's designation and context as a commercial site.

Consistency with the Municipal Development Plan

The subject parcel is designated "Commercial" on Map 9 – Land Use Concept of the Municipal Development Plan (MDP). The MDP explicitly supports a broad range of commercial activities in these areas, including retail, office, hospitality, entertainment, and complementary uses. Section 2.6 of the MDP describes commercial character areas as locations intended to accommodate concentrations of uses that serve a community-wide customer base and benefit from exposure and access along major roadways. The proposed development aligns with this intent through its commercial format and two-storey building design. The site is bounded by an Arterial road to the west and collector roads to the north and east.

Further, the proposal is consistent with the following MDP policies:

- Section 4.13.1, which encourages commercial uses in locations identified on Map 9, including retail, hospitality, and entertainment uses;
- Section 4.13.2, which directs commercial uses to locate along major roadways to ensure access and minimize land use conflicts. The site is located adjacent to 50th Street (Arterial) and Coloniale Way (Major Collector);
- Section 4.13.4, which encourages commercial development oriented to public roadways to create a pedestrian-friendly streetscape;
- Section 4.13.5, which encourages multi-storey commercial buildings that make efficient use of land and infrastructure;
- Section 5.1.6, which encourages storefront commercial businesses and a diversity of commercial services.
- The Development Authority also considered Section 4.13.6 of the MDP, which requires that commercial developments be supported by appropriate servicing, access, and infrastructure. These matters were addressed through the development permit review process and supporting technical submissions, with all required infrastructure to be provided at the developer's expense.

Outline Plan Consistency

- The Coloniale Estates Outline Plan, approved by Council on February 12, 2008, designates the subject parcel for Commercial development. The approved development is consistent with this designation and implements the long-standing land use intent for the site.

Exercise of Discretion and Planning Rationale

While discretionary uses require careful consideration, they remain expressly contemplated uses within the Land Use Bylaw. The role of the Development Authority is to assess whether the proposed development meets the applicable regulatory framework and is reasonably compatible with its context. In this case:

- The development complies with all applicable Land Use Bylaw regulations;
- No variances were required;
- The uses align with the MDP and Outline Plan land use designations;
- The site is located within a planned commercial area designed to accommodate a range of commercial and service-oriented activities; and
- The scale, form, and design of the development are consistent with the intended character of the area.

Based on these considerations, the Development Authority determined there was no valid land use planning basis to withhold approval of the proposed discretionary uses.

Public Feedback and Consideration by the Development Authority

Because the application included Discretionary Uses, pre-decision public notification was undertaken in accordance with Section 5.16.1 of Land Use Bylaw 944-19. A notification sign was posted on the subject site from November 7 to December 5, 2025. During this period, several submissions were received, indicating a high level of neighbourhood interest.

The Development Authority reviewed and considered all submissions prior to rendering a decision. The feedback was predominantly opposed, with recurring themes focused on traffic and road safety, scale and intensity of development, parking adequacy, and neighbourhood character and compatibility, particularly given the site's location adjacent to residential uses, a school, and recreational facilities.

Throughout the development review process, and prior to the receipt of public feedback, the Development Authority worked with the applicant to address key planning considerations associated with the proposal. With respect to traffic and road safety, the City required the submission of a Traffic Impact Assessment to evaluate anticipated traffic generated by the development. The purpose of the Traffic Impact Assessment was to determine whether the surrounding road network could accommodate the proposed development and whether off-site improvements would be required. The assessment concluded that the surrounding road network can support the anticipated traffic volumes associated with the development.

With respect to development scale, intensity, and site orientation, the proposal meets or exceeds all applicable Land Use Bylaw requirements related to setbacks, site coverage, parking provision, and landscaping. The orientation of the buildings is consistent with the applicable statutory policy framework and supports pedestrian activity along Rue Bouchard. Through the review process, the City worked with the applicant to enhance the Coloniale Way frontage through upgraded building materials and increased glazing.

Neighbourhood character and compatibility were also assessed through review against the Beaumont Urban Design Guidelines and through the Design Review Committee process. As part of this review, the City worked with the applicant to improve building articulation, material quality, and glazing, with particular attention to the facades along Coloniale Way.

Redacted feedback, including the "What We Heard" summary, was shared with the applicant. While the applicant did not make further changes in response to the public submissions, this was considered in the context of the issues already identified and addressed through the technical, policy, and design review processes undertaken by the Development Authority.

In considering the public feedback, the Development Authority distinguished between matters that are relevant to a development permit decision under the Land Use Bylaw and matters that fall outside the Development Authority's jurisdiction. While concerns related to traffic operations, parking, and interface with adjacent residential properties were considered as part of the discretionary use compatibility analysis, other comments, such as preferences for alternative land uses, opposition to commercial development in principle, or broader infrastructure and policy matters, are more appropriately addressed through statutory planning processes or

land use district amendments.

After assessing the public feedback alongside the applicable policy framework, the Development Authority concluded that:

- the subject site is designated Commercial in the Municipal Development Plan and Outline Plan;
- the proposed uses are contemplated by the Land Use Bylaw as permitted or discretionary uses within the district;
- the development complies with all applicable Land Use Bylaw regulations, with no variances requested or approved; and
- the proposed development, as designed and conditioned, is reasonably compatible with surrounding land uses.

While the volume and strength of opposition was acknowledged, the Development Authority's role is to apply the Land Use Bylaw and statutory plans and determine reasonable compatibility of the Discretionary Uses. On this basis, the Development Authority determined that the public feedback, when weighed against the regulatory framework and planning context, did not provide a sufficient land use planning basis to refuse the application.

5. Subdivision and Development Appeal Board Authority and Scope of Consideration

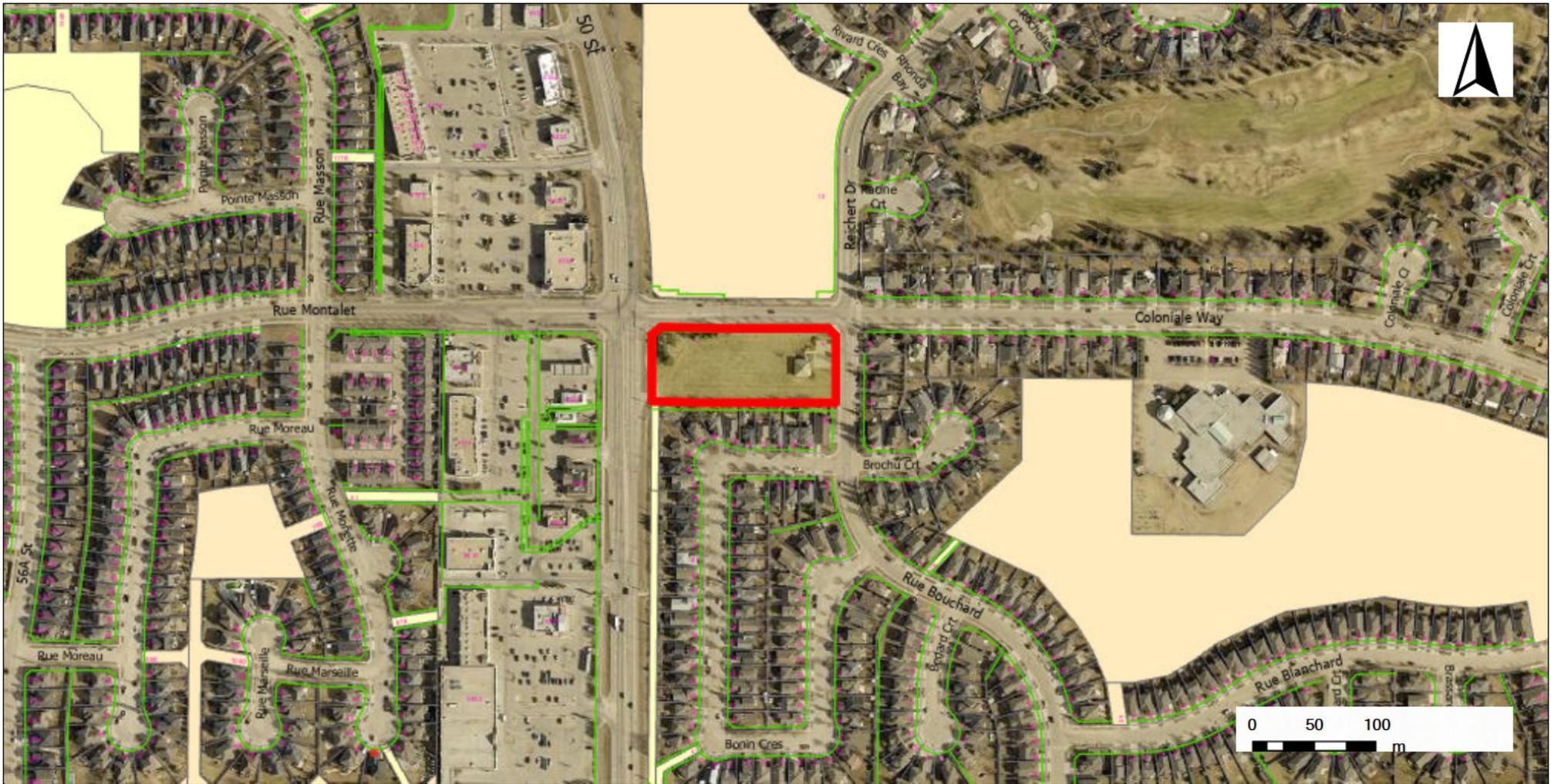
In issuing the Development Permit, the Development Authority exercised its authority under the Municipal Government Act and Land Use Bylaw 944-19 to approve discretionary uses where they are determined to be reasonably compatible with surrounding development and consistent with applicable statutory plans.

This report is intended to outline the regulatory framework, the considerations reviewed by the Development Authority, and the rationale supporting the decision that was made. The Development Authority acknowledges that the Subdivision and Development Appeal Board, in hearing the appeal, may consider the evidence presented and may confirm, revoke, or vary the Development Permit in accordance with its authority under the Municipal Government Act.

From the Development Authority's perspective, the permit was issued based on compliance with the Land Use Bylaw, alignment with the Municipal Development Plan and Outline Plan, and a determination that the proposed development is reasonably compatible with the surrounding context. The Development Authority has not identified any regulatory or planning deficiencies in the approval that would warrant reconsideration of the decision.

6. Exhibits

1. Location Map
2. Zoning Regulations
3. Development Permit Application
4. Development Permit Decision
5. MDP Excerpts
6. Coloniale Estates Outline Plan
7. What We Heard & Did Report
8. Site Photographs
9. Development Authority Presentation



City of Beaumont
 5600 49 Street
 Beaumont, AB
 T4X 1A1

9 Coloniale Way

9 COLONIALE WAY

Application No.
 2025-077

Mapped By:
 Craig Thomas

Checked By:

Numeric Scale
 1: 4649

Date
 February 10, 2026

Projected Coordinate System
 CANADA NAD 83-3TM 114

Legend

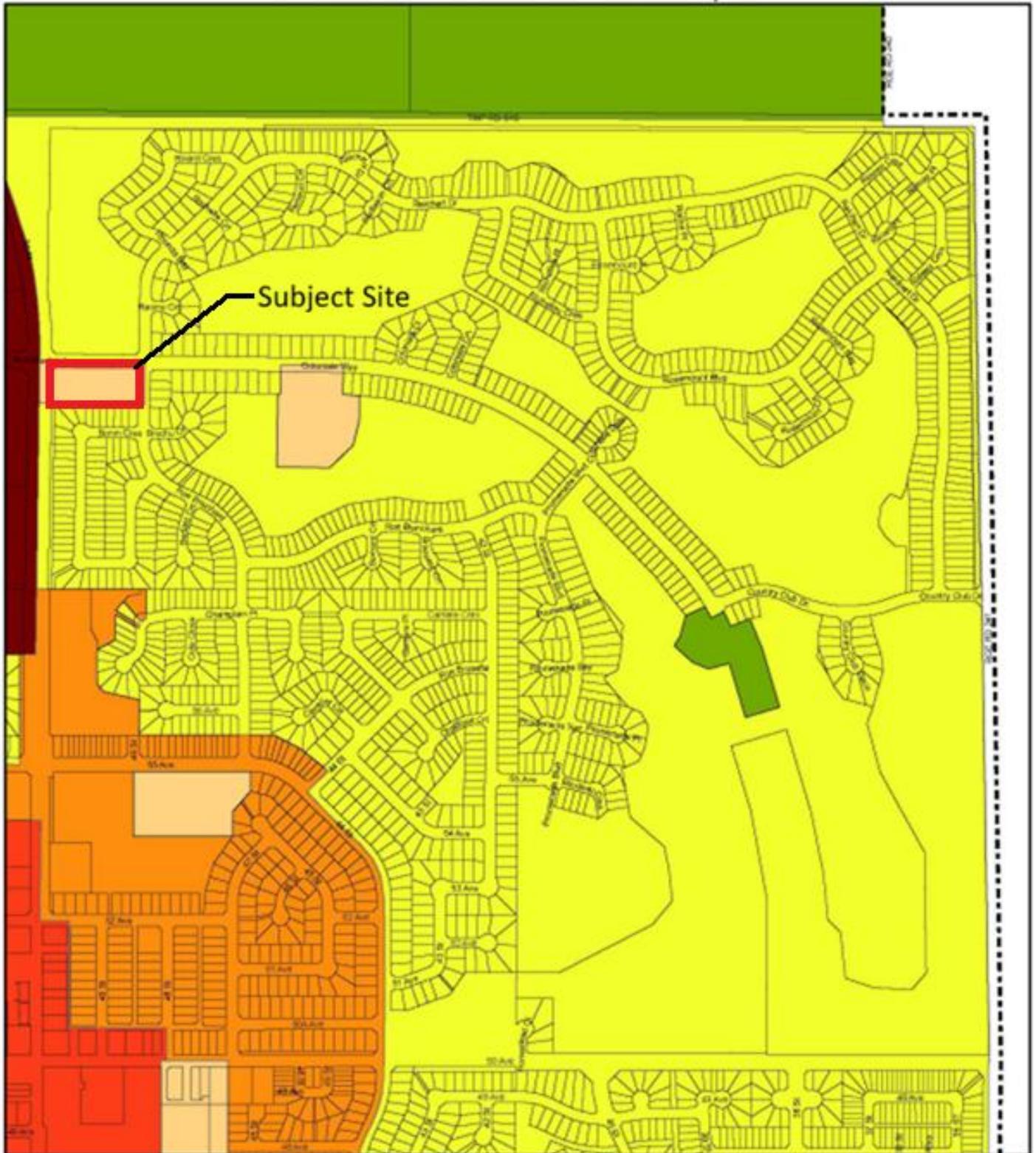


Location:



Registered Parcels

Beaumont Land Use District Map 6

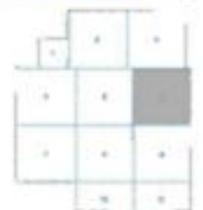


Legend

- Agricultural Holdings District (AH)
- Conventional Neighbourhood District (CN)
- Mature Neighbourhood District (MN)
- Main Street District (MS)
- Commercial District (C)
- Business Light Industrial District (BLI)
- Municipal Boundary



Date Updated: July 27, 2022





Our Zoning Blueprint:

BEAUMONT LAND USE BYLAW

Bylaw 944-19

This is an interactive PDF.

Click on words [highlighted in green](#) to jump to that section of the document.

[AGRICULTURAL HOLDINGS DISTRICT \(AH\)](#)



Hover your mouse over words [highlighted in blue](#) to view pop-up definitions

An area to accommodate a vehicle while being loaded or unloaded.

[Loading Space](#)



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PART 1: PURPOSE & AUTHORITY

1.1 GENERAL PURPOSE

The purpose of this [Land Use Bylaw](#), hereby known as “[Our Zoning Blueprint](#)”, is to enable sustainable development based on the principles of Beaumont’s Municipal Development Plan: [Our Complete Community](#). To accomplish this, [Our Zoning Blueprint](#) organizes sustainable development patterns by character Districts based on [Frontage](#) types and with the knowledge that development decisions made today will impact future generations.

In order to ensure this purpose is achieved, all planning applications, including land [use](#), [subdivision](#), and [development permit](#) applications, will be evaluated using the principles of [Our Complete Community](#).

1.2 AUTHORITY

- 1.2.1 The action of Beaumont, hereby known as “the [Municipality](#)”, in the adoption of [Our Zoning Blueprint](#) is authorized under the [Municipal Government Act](#), as amended.
- 1.2.2 The adoption of [Our Zoning Blueprint](#) is necessary to promote the health, safety, and general welfare of the citizens of the [Municipality](#) and to assist in the coordinated, efficient, and economical development of the [Municipality](#). [Our Zoning Blueprint](#) is implemented in order to advance the objectives and policies of [Our Complete Community](#), the [General Design Standards](#), and any other applicable statutory or non-statutory plan.
- 1.2.3 No person shall commence any development within the [Municipality](#) except in conformity with [Our Zoning Blueprint](#).

1.3 APPLICABILITY

- 1.3.1 [Our Zoning Blueprint](#) shall apply to all lands contained within the corporate limits of the [Municipality](#).
- 1.3.2 [Our Zoning Blueprint](#) shall be consistent with [Our Complete Community](#) and shall be applied in a manner that serves to implement other statutory plans and master plans adopted by the [Municipality](#). Should a property be redistricted or a district in [Our Zoning Blueprint](#) be amended, it shall conform to [Our Complete Community](#).
- 1.3.3 The provisions of [Our Zoning Blueprint](#), when in conflict, shall take precedence over those of other municipal bylaws or regulations.

PART 1: PURPOSE & AUTHORITY

- 1.3.4 Any federal, provincial and regional legislation, regulations, approval processes, licensing, or permitting shall be applied to, and take precedence over, development applications as required.
- 1.3.5 If any provision of *Our Zoning Blueprint* is held by a court of competent jurisdiction to be invalid, then all other provisions shall remain valid and enforceable.
- 1.3.6 Where the proposed [use](#) or [structure](#) does not comply with any federal, provincial or other municipal legislation or with the conditions of any caveat, covenant, easements, instrument, [building](#) scheme or agreement affecting the land or [building](#), the [Development Authority](#) may refuse to grant a [Development Permit](#).
- 1.3.7 Neither [Council](#) nor the [Development Authority](#) are required to examine land title(s) or make inquiry to discover whether or not the [use](#) of a [building](#) or land is affected by any federal, provincial, or other municipal legislation or condition of any easement, covenant, [building](#) scheme, or agreement.
- 1.3.9 Any reference in *Our Zoning Blueprint* to other legislation or documents shall be a reference to the bylaw or legislation then in effect and shall include all amendments and any other successor legislation.

1.4 TRANSITION

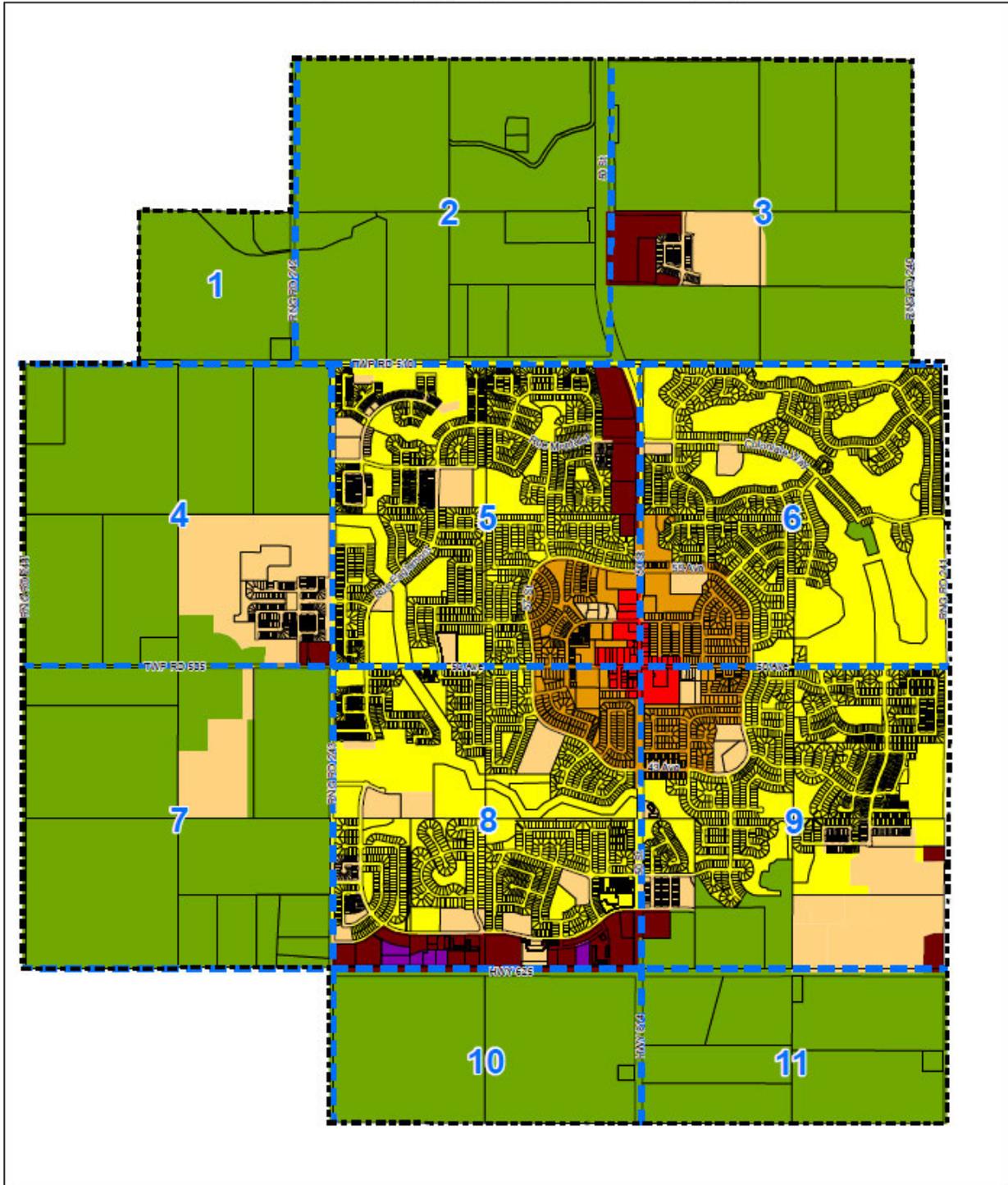
- 1.4.1 *Our Zoning Blueprint* shall come into effect upon the date of its third reading and it is signed by the Chief Administrative Officer and Chief Elected Official.
- 1.4.2 Applications for [subdivision](#) and development which were submitted prior to *Our Zoning Blueprint* coming into force shall be evaluated under the provisions of Bylaw No. 796-12, as amended.

2.1 LAND USE MAP

- 2.1.1 The [Municipality](#) is hereby divided into the following [land use districts](#):
- a) [Agricultural Holdings District \(AH\)](#);
 - b) [Conventional Neighbourhood District \(CN\)](#);
 - c) [Integrated Neighbourhood District \(IN\)](#);
 - d) [Mature Neighbourhood District \(MN\)](#);
 - e) [Main Street District \(MS\)](#);
 - f) [Commercial District \(C\)](#); and
 - g) [Business Light Industrial District \(BLI\)](#).
- 2.1.2 The [land use districts](#) listed in Section 2.1.1 are delineated on the maps in this Section, which shall be known as the “Land Use Maps”.
- 2.1.3 The Land Use Map may be amended or replaced by bylaw from time to time.
- 2.1.4 In the event that a dispute should arise over the precise location of a boundary of any [land use district](#) as shown on the Land Use Maps, the [Development Authority](#) shall decide thereon.

PART 2: MAPS

Beaumont Land Use Districts Key Map



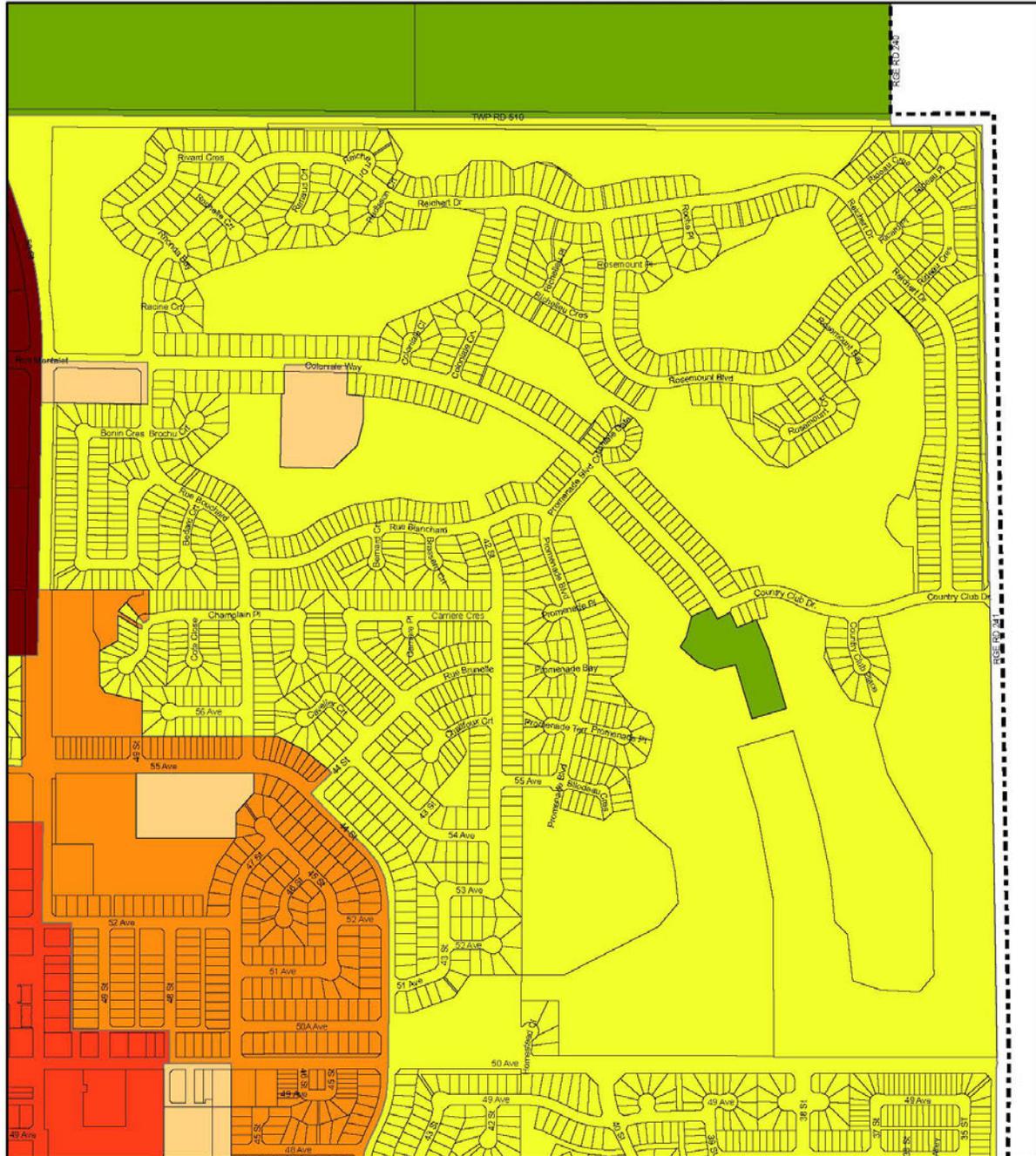
Legend

- | | | |
|---|--|--|
| ■ Agricultural Holdings District (AH) | ■ Mature Neighbourhood District (MN) | ■ Business Light Industrial District (BLI) |
| ■ Conventional Neighbourhood District (CN) | ■ Main Street District (MS) | ■ Municipal Boundary |
| ■ Integrated Neighbourhood District (IN) | ■ Commercial District (C) | |



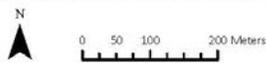
PART 2: MAPS

Beaumont Land Use District Map 6

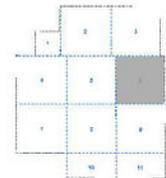


Legend

- Agricultural Holdings District (AH)
- Conventional Neighbourhood District (CN)
- Integrated Neighbourhood District (IN)
- Mature Neighbourhood District (MN)
- Main Street District (MS)
- Commercial District (C)
- Business Light Industrial District (BLI)
- Municipal Boundary



Date Updated: July 27, 2022

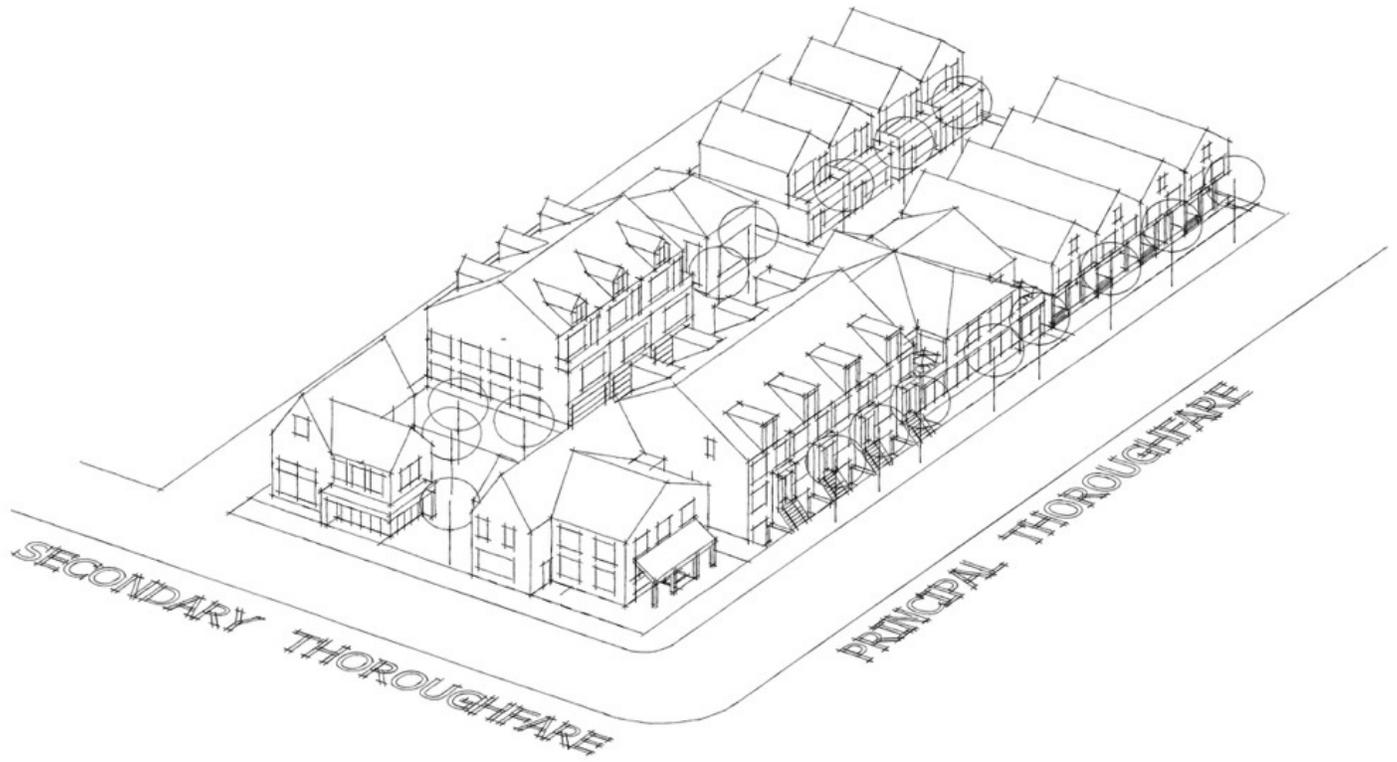


3.1 GENERAL OVERVIEW

- 3.1.1 The [land use districts](#) within the [Municipality](#) include the following:
- a) [Agricultural Holdings District \(AH\)](#);
 - b) [Conventional Neighbourhood District \(CN\)](#);
 - c) [Integrated Neighbourhood District \(IN\)](#);
 - d) [Mature Neighbourhood District \(MN\)](#);
 - e) [Main Street District \(MS\)](#);
 - f) [Commercial District \(C\)](#); and
 - g) [Business Light Industrial District \(BLI\)](#).
- 3.1.2 Each [land use district](#) is reflective of a unique physical and social character as illustrated in each [land use district](#).
- 3.1.3 In addition to the regulations specified within each [land use district](#), all parts of [Our Zoning Blueprint](#) apply, as appropriate, to all lots within the [Municipality](#).

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)



PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

3.4.2 Uses

The [uses](#) identify whether a [use](#) is permitted, discretionary, or not allowed in this [land use district](#).

Definitions are provided in Part 6.

| | | | |
|--|---|--|-------|
| AGRICULTURE USES | | Retail & Service - General | P |
| Agriculture - General | - | Retail & Service - Large | - |
| Agriculture - Intensive | - | Show Home | P |
| Agriculture - Urban | P | INDUSTRIAL USES | |
| Cannabis Production and Processing | - | Industrial - Medium | - |
| Medical Cannabis Production | - | Industrial - Light | - |
| RESIDENTIAL USES | | Recreational Vehicle Storage | - |
| Dwelling Unit(s) | P | Wash Station | - |
| Mobile Home | - | INSTITUTIONAL USES | |
| Temporary Dwelling Unit(s) | D | After Life Care | D |
| LODGING USES | | Cemetery | - |
| Bed & Breakfast | D | Culture | P |
| Campground | D | Education | D |
| Hotel / Motel | - | Emergency Service Training Facility | - |
| BUSINESS USES | | Government | P |
| Arts & Crafts | D | Hospital | P |
| Home Based Business - Major | D | Human Services | D |
| Home Based Business - Minor | P | Motor Vehicle Training and Research Facility | - |
| Office | P | Recreation - Active | P |
| COMMERCIAL USES | | Recreation - Passive* | P |
| Adult Entertainment | - | Parking Lot with no associated Use | - |
| Drive Through Facility | - | Special Events | D |
| Entertainment Establishment | D | OTHER USES | |
| Gas Station | - | Accessory Building or Structure | P |
| Golf Course | D | Public Utility* | P |
| Kennel | - | Excavation, Stripping & Grading | D |
| Restaurant / Café | D | Private Utility* | P |
| Restricted Substance Retail | - | Sign (as per Part 4) | P / D |
| | | Temporary Development | D |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

*No *Development Permit* required

P = [Permitted Use](#)

D = [Discretionary Use](#)

- = Not allowed

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

3.4.3 Use Standards

-
- | | |
|--|---|
| a) Bed & Breakfast | <ol style="list-style-type: none">i. The use shall be restricted to dwelling unit(s);ii. The character or external appearance of the building shall not be changed, except where minimal alterations are required for the use;iii. Nuisances, in the opinion of the Development Authority, shall not be created by way of noise, parking, or traffic generation; andiv. 1 sign shall be permitted. |
| <hr/> | |
| b) Campground | <ol style="list-style-type: none">i. Campgrounds shall be in accordance with the <i>Our Place and Play Master Plan</i>;ii. Where possible, existing topography and natural features such as tree stands, shall be integrated in the site design;iii. The whole perimeter of the site shall be buffered sufficiently at the discretion of the Development Authority;iv. No outdoor speakers are permitted; andv. Nuisances, in the opinion of the Development Authority, shall not be created by way of noise, parking, or traffic generation. |
| <hr/> | |
| c) Dwelling Unit above a detached garage | <p>Where a dwelling unit is located above a detached garage, windows shall be placed and sized such that they minimize direct views of adjacent lot(s) through one or more of the following:</p> <ol style="list-style-type: none">i. Off-setting window placement to limit direct view into a window of an adjacent site;ii. Strategic placement of windows in conjunction with landscaping features; and/oriii. Placing larger windows to face a lane, flanking public roadway or other dwelling on the same site. |
| <hr/> | |
| d) Home Based Business - Major | <ol style="list-style-type: none">i. Up to 10 clients per day are permitted;ii. May include a day home;iii. 1 non-illuminated sign shall be permitted;iv. May include outdoor activities that do not cause a nuisance for adjacent lots, in the opinion of the Development Authority; andv. No more than two (2) employees shall be in attendance at any one time. |
-

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

| | |
|--|---|
| e) Home Based Business - Minor | i. No client visits are permitted; ii. The residential character of the building shall not be affected; iii. Shall be contained within a building ; iv. No signs are permitted; and v. No accessory structures can be utilized for the purpose of the use . |
| f) Restaurant / Café | Outdoor speakers shall comply with any noise restrictions set by the Municipality . |
| g) Temporary Dwelling Unit | Cannot include recreational vehicles or and shall be greater than 10m ² . |
| h) Education | All Child Care facilities, shall provide an on-site drop-off area that is marked as a loading zone only suitable to accommodate traffic associated with the use and limit any impact on the surrounding community. |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

3.4.4 Block / Subdivision Standards

| | |
|--|--|
| a) Block Length | Max 240 m |
| b) Block & Subdivision Standards | <ol style="list-style-type: none">i. To create a pedestrian network throughout Beaumont a mid-block pedestrian walkway shall be dedicated as a right-of-way where the block length exceeds 240 m. The location of the right-of-way cannot be located within 75 m of the ends of the block length. With the addition of the right-of-way, the new block length shall not exceed 240m. At the discretion of the Development Authority, lots adjacent to arterial roads or highways may not be required to include a pedestrian right-of-way, or where the topographic changes, existing buildings or other natural or man-made obstructions prevent such access, and where strict compliance would pose a safety hazard.ii. Block standards may be varied to conform to natural features, transportation rights-of-way, parks or open space, existing utilities, or other similar constraints.iii. A minimum of 40% of net developable area within this land use district shall have rear lanes as per the area structure plan, neighbourhood structure plan, and / or outline plan. Net developable area does not include lots adjacent to storm water utilities, arterial roads, highways, or similar circumstances, including but not limited to public utility lots, municipal reserves, and environmental reserves.iv. Shadow plans of future phases of development shall be required to demonstrate how the minimum percentage of rear lanes is achieved. |
| c) Lot Width | <ol style="list-style-type: none">i. Min 4.9 m to max 15 m except for multi-unit buildings or institutional uses where the lot width may be increased, at the discretion of the Development Authority.ii. Min 4.2 m for internal lots of multi-attached buildings where access is from a lane at the rear of the property. |
| d) Single Side Yard | Blocks intended for Single Side Yard Dwellings with primary access from the principal frontage shall not be across the thoroughfare from lots that are intended for the purposes of Single Side Yard Dwellings with primary access from the principal frontage. |

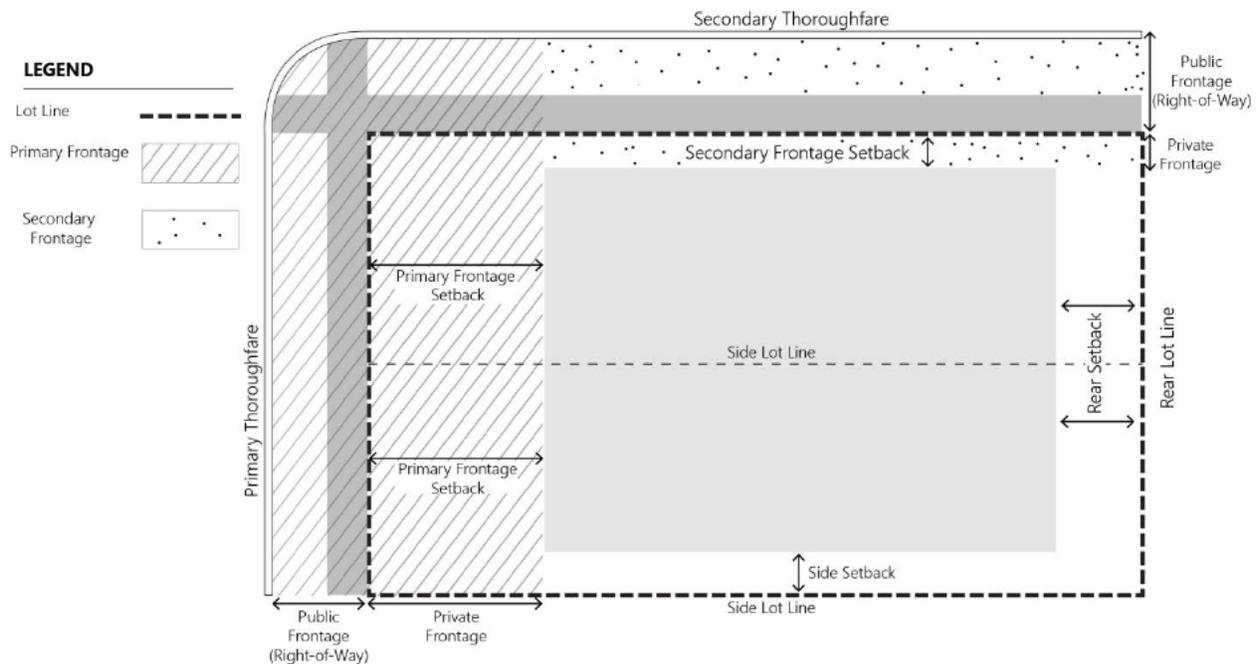
PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

3.4.5 Residential Density

| | |
|------------|---|
| a) Minimum | <ul style="list-style-type: none">i. For built up areas: 1 dwelling unit(s) per lotii. For planned development: 35 units per net hectare or as per the applicable area structure plan, neighbourhood structure plan, or outline plan. Shadow plans of future phases of development shall be required to demonstrate how the minimum density is achieved. |
| b) Maximum | 2 dwelling units per lot or as per the applicable Area Structure Plan, Neighbourhood Structure Plan, or Outline Plan |

3.4.6 Building Placement Standards



| | |
|---|--|
| a) PRINCIPAL BUILDINGS | |
| <ul style="list-style-type: none">i. Principal Frontage Setback | <ul style="list-style-type: none">i. 3 m Minimumii. Principal frontage setbacks shall be different from the adjacent lot for single-detached dwellings. |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

a) PRINCIPAL BUILDINGS

| | | |
|------|--|---|
| ii. | Secondary Frontage Setback | Min 2.4 m when adjacent to a public roadway or 1.2 m when adjacent to a lane to max 4 m |
| iii. | Side Yard Setback | Min 1.2 m to max 4 m except for attached buildings where side yard setback is 0 m on the attached side |
| iv. | Single Side Yard Standards | Min 1.5 m setback where other side yard is 0 m. A private maintenance easement shall be registered on titles adjacent to the zero lot line that provide a 0.30 m eave encroachment easement where no eave shall be closer than 0.90 m to the eave of the adjacent building ; a 0.60 m footing encroachment easement, and provides sufficient access for maintenance of both properties. All utilities and lot grading shall be to the satisfaction of the Development Authority . |
| v. | Rear Yard Setback | Min 1.2 m |
| vi. | Lot Coverage | i. Max 55% (including accessory building lot coverage as per Section 3.4.6 (b) (v)) ii. Max 60%, (including accessory building lot coverage as per Section 3.4.6 (b) (v)) for internal lots of multi-attached buildings. |

b) ACCESSORY BUILDINGS

| | | |
|------|--|---|
| i. | Principal Frontage Setback | Min 3 m and no closer than the principal building |
| ii. | Secondary Frontage Setback | Min 2.4 m |
| iii. | Side Yard Setback | Min 1.2 m, except for attached buildings where side yard setback is 0 m |
| iv. | Rear Yard Setback | Min 1.2 m |
| v. | Lot Coverage | Max 15%, except for multi-attached accessory buildings on internal lots may be increased to a maximum of 18% at the discretion of the Development Authority . |

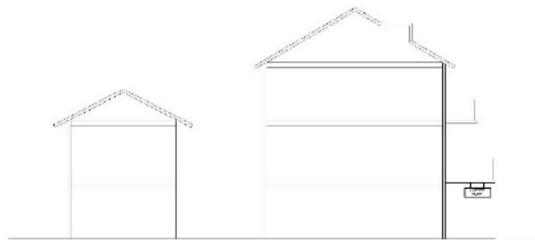
PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

| c) ADDITIONAL SITE STANDARDS | |
|------------------------------|--|
| i. Corner Visibility | No building , structure , fence , or soft landscaping that will obstruct vision above 0.6 m in height shall be located within the corner cut area. |
| ii. General Safety | Design elements that allow for casual surveillance, not including digital surveillance, are expected to be included in the design. These elements may include, but are not limited to, door placement, large window areas, high quality interior and exterior lighting, a physical layout that reduces the vulnerability of pedestrians, the placement and use of soft landscaping that limits areas of concealment, and integrating the pedestrian network with building entrances. |
| iii. Lighting | All permanently installed lighting shall be directed downward, be shielded in a manner to not be directed to adjacent lots, and shall not, in the opinion of the Development Authority adversely impact safety. All permanently installed lighting shall be compliant with International Dark-Sky Association requirements. |
| iv. Vehicles | Dismantled or wrecked vehicles and commercial vehicles are prohibited from parking in a frontage . All vehicles shall be parked on a parking stall . |
| v. Solid Waste | All non-residential uses shall provide a solid waste storage area suitable for 2- or 3-stream waste diversion for the intended use and designed to the satisfaction of the Development Authority . All areas shall be located at the side or rear of a lot , screened from view and accessed from a public roadway or lane . |

3.4.7 Building Profile Standards

- a) [Principal Building Height](#) to [Eave](#) Max 4 [storeys](#)



PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

b) Accessory [Building Height](#) to [Eave](#) Max 2 [storeys](#) but in any event, no taller than the [principal building](#).

c) Design Standards

- i. The finish and appearance of all [buildings](#) on the [lot](#), including accessory [buildings](#), shall complement the other [structures](#) and natural features located on the same [lot](#).
- ii. The design of dwellings must ensure individuality and a variety of dwellings. This will require consideration of the exterior treatment of materials, textures, rooflines and wall openings on the same side of the public roadway, as well as directly across the public roadway from one another. Design variability for the [principal](#) dwelling shall follow an A B C D A pattern along the [block](#).
- iii. Mechanical equipment shall be screened or incorporated into the roof envelope, where appropriate.
- iv. [Buildings](#) on prominent [corner lots](#) shall have the same materials and architectural details on all street exposures. Entrances may be located near the corner.
- v. No tarpaulin [structures](#).
- vi. For multi-attached buildings, including townhouses or buildings with three or more principal dwellings, the façade must incorporate at least two design techniques or features to reduce the perception of massing, eliminate large blank walls, and enhance design variation. Design techniques or features may include: variations in rooflines; vertical or horizontal building wall projection or recessions; visual façade breaks into smaller sections; features such as windows, balconies, or porches; use of a combination of finishing materials; or other similar techniques or features.

d) Allowable [Projections](#) into [Setbacks](#) Gallery, [balcony](#), [sign](#), awning, [accessibility features](#), [eave](#), cantilever, chimney / fireplace

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

3.4.8 Frontage Type Standards

Frontage Type Standards shall apply to each principal frontage for all uses except institutional uses where the character of each frontage type shall be maintained, but variations to the entrance feature characteristics do not apply. Entrance features are any pedestrian access / egress to a building.

a) FRONT ATTACHED GARAGE

A frontage wherein a driveway and attached garage are located with a front entrance feature perpendicular to the principal thoroughfare.

| Section View | Plan View | Entrance Feature characteristics: | | | | |
|----------------------|--|---|---|---|-----------|---|
| | | i. Width <i>From outside edge of <u>building</u> element included in the <u>entrance feature</u></i> | ii. Depth <i>Exterior foundation projection of the <u>entrance feature</u></i> | iii. Height <i>From <u>grade</u> to highest point of the <u>entrance feature</u></i> | | |
| Additional Standards | <ul style="list-style-type: none"> iv. <u>Entrance features</u> shall be wholly visible from the <u>principal thoroughfare</u>. v. A minimum of 1 tree is required per <u>lot</u>. vi. All driveways shall extend a minimum of 6 m from the <u>lot line</u> to the garage foundation. vii. Driveways shall be no wider than the garage. viii. Where possible, curb cut widths shall be minimized. ix. Where possible, driveways shall be paired. | | | 1.2 m min | 1.2 m min | - |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

b) COMMON YARD

A planted frontage wherein the façade is set back from the front lot line. The principal frontage remains unfenced and is visually continuous with adjacent yards, supporting a common landscape.

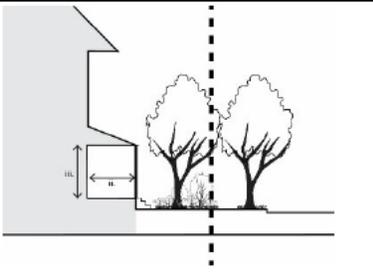
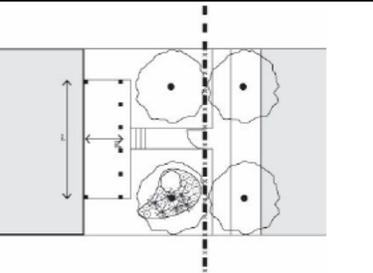
| Section View | Plan View | Entrance Feature characteristics: | | | | | | | | | |
|---|---|--|----------|-----------|-------------|---|---|--|-----------|---|---|
| | | <table border="0"> <tr> <td data-bbox="990 766 1185 840">i. Width</td> <td data-bbox="1185 766 1364 840">ii. Depth</td> <td data-bbox="1364 766 1546 840">iii. Height</td> </tr> <tr> <td data-bbox="990 840 1185 913"><i>From outside edge of <u>building element</u> included in the <u>entrance feature</u></i></td> <td data-bbox="1185 840 1364 913"><i>Exterior foundation <u>projection</u> of the <u>entrance feature</u></i></td> <td data-bbox="1364 840 1546 913"><i>From <u>grade</u> to highest point of the <u>entrance feature</u></i></td> </tr> <tr> <td data-bbox="990 913 1185 976">1.2 m min</td> <td data-bbox="1185 913 1364 976">-</td> <td data-bbox="1364 913 1546 976">-</td> </tr> </table> | i. Width | ii. Depth | iii. Height | <i>From outside edge of <u>building element</u> included in the <u>entrance feature</u></i> | <i>Exterior foundation <u>projection</u> of the <u>entrance feature</u></i> | <i>From <u>grade</u> to highest point of the <u>entrance feature</u></i> | 1.2 m min | - | - |
| i. Width | ii. Depth | iii. Height | | | | | | | | | |
| <i>From outside edge of <u>building element</u> included in the <u>entrance feature</u></i> | <i>Exterior foundation <u>projection</u> of the <u>entrance feature</u></i> | <i>From <u>grade</u> to highest point of the <u>entrance feature</u></i> | | | | | | | | | |
| 1.2 m min | - | - | | | | | | | | | |
| Additional Standards | <ul style="list-style-type: none"> iv. A minimum of 2 trees are required per <u>lot</u> except for lots less than 6 m wide where 1 tree, or 3 shrubs, is required per <u>lot</u>. v. Where a <u>porch</u> is included, it shall project at least 1.2 m from the front <u>façade</u>, not including stairs, and shall be equal to or greater than the width of the <u>entrance feature</u> as per Section 3.4.8 (b) (i). | | | | | | | | | | |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

c) PORCH & FENCE

A planted frontage where the façade is set back from the front lot line with an attached porch. A fence at the front lot line provides separation from the public realm.

| Section View | Plan View | Entrance Feature characteristics: | | |
|--|---|---|---|--|
|  |  | i. Width | ii. Depth | iii. Height |
| Additional Standards | <ul style="list-style-type: none"> iv. A minimum of 2 trees are required per <u>lot</u>. v. Front fences shall be no higher than 1 m. | <p><i>From outside edge of <u>building</u> element included in the <u>entrance feature</u></i></p> <p>1.2 m min</p> | <p><i>Exterior foundation <u>projection</u> of the <u>entrance feature</u></i></p> <p>1.2 m min</p> | <p><i>From <u>grade</u> to highest point of the <u>entrance feature</u></i></p> <p>2.4 m min</p> |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

d) ALTERNATE HEIGHT ENTRANCE

A frontage wherein the façade is setback back from the lot line by an elevated or sunken entrance. This frontage type buffers uses from urban sidewalks and removes the private yard from public encroachment. Entrances may be suitable for conversion to outdoor restaurants / cafés or similar uses. This frontage shall be designed to incorporate accessibility features.

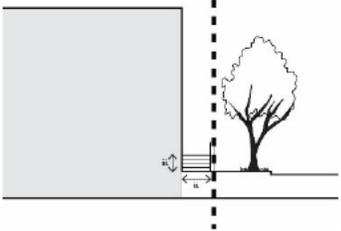
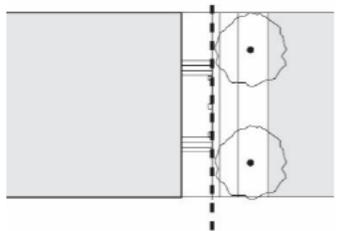
| Section View | Plan View | Entrance Feature characteristics: | | | | |
|----------------------|--|---|--|---|--|----------------------------|
| | | i. Width <i>From outside edge of <u>building element</u> included in the <u>entrance feature</u></i> | ii. Depth <i>Exterior <u>projection of the entrance feature</u></i> | iii. Height <i>From <u>grade</u> to highest point of the <u>entrance feature</u></i> | | |
| Additional Standards | iv. Fences shall be provided between the <u>thoroughfare</u> and the <u>entrance feature</u> that meets safety code requirements. v. Trees shall be incorporated in any area of the <u>frontage</u> that is not hardscaped. Every effort should be made to accommodate trees, however, where enough space cannot be accommodated alternate <u>soft landscaping</u> may be used, at the discretion of the <u>Development Authority</u> . | | | 1.2 m min | 2 m max (not including stairs or <u>accessibility features</u>) | Aligned with floor heights |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

e) STOOP

A frontage where the first storey is elevated from grade. This frontage type is typically used for buildings where the elevation provide additional privacy for windows. As such, the entrance feature should be clearly welcoming and accommodating to reduce the perception of separation as a result of the change in grade.

| Section View | Plan View | Entrance Feature characteristics: | | |
|--|--|---|--|---|
|  |  | i. Width <i>From outside edge of <u>building</u> element included in the <u>entrance feature</u></i> | ii. Depth <i>Exterior foundation <u>projection</u> of the <u>entrance feature</u></i> | iii. Height <i>From <u>grade</u> to highest point of the <u>entrance feature</u></i> |
| Additional Standards | iv. <u>Projections</u> , such as awnings, that overhang the <u>entrance feature</u> may be considered, but shall not be more than 4 m from the floor height. | | | |

| | | |
|---|---------|---------------------------|
| 1.8 m min (not including stairs or <u>accessibility features</u>) | 2 m max | Aligned with floor height |
|---|---------|---------------------------|

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

f) SHOPFRONT

A **frontage** where the **entrance feature** is at sidewalk **grade** and where the **façade** has several windows on the ground floor with an awning or similar **structure** projecting over the entrance.

| Section View | Plan View | Entrance Feature characteristics: | | |
|--------------|-----------|--|--|--|
| | | i. Width <i>Percentage of lot width</i> | ii. Depth <i>Exterior foundation projection of the entrance feature</i> | iii. Height <i>From grade to lowest point of the awning or overhang</i> |
| | | 30% min | 1.5 m min | 3.05 m max |

Additional Standards

- iv. Ground floor **uses** are limited to lodging, commercial, and institutional **uses**.
- v. Where appropriate, a **projection sign** shall be integrated with the **entrance feature**.
- vi. **Hard landscaping** shall be contiguous and seamlessly integrated with the public sidewalk with no **grade** adjustments.
- vii. A minimum of 50% of the ground floor **façade** shall be composed of non-glazed windows and doors.
- viii. Sidewalk Cafés may be incorporated as per Beaumont's *Sidewalk Café Guidelines*.

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

g) GALLERY

A frontage where the entrance feature is an attached cantilevered eave or lightweight colonnade overhanging the entrance. The entrance feature may overhang, or encroach on, the lot line where pedestrian movement is not hindered.

| Section View | Plan View | Entrance Feature characteristics: | | | | | | |
|--|--|---|--|--|---|---------|---------|-----------|
| | | <table border="0"> <tr> <td data-bbox="990 745 1153 1033">i. Width <i>Percentage of lot width</i></td> <td data-bbox="1153 745 1315 1033">ii. Depth <i>Exterior foundation projection of the entrance feature</i></td> <td data-bbox="1315 745 1542 1033">iii. Height <i>From grade to highest point of the entrance feature</i></td> </tr> <tr> <td data-bbox="990 861 1153 903">30% min</td> <td data-bbox="1153 861 1315 903">3 m min</td> <td data-bbox="1315 861 1542 903">3.5 m min</td> </tr> </table> | i. Width <i>Percentage of lot width</i> | ii. Depth <i>Exterior foundation projection of the entrance feature</i> | iii. Height <i>From grade to highest point of the entrance feature</i> | 30% min | 3 m min | 3.5 m min |
| i. Width <i>Percentage of lot width</i> | ii. Depth <i>Exterior foundation projection of the entrance feature</i> | iii. Height <i>From grade to highest point of the entrance feature</i> | | | | | | |
| 30% min | 3 m min | 3.5 m min | | | | | | |

Additional Standards

- iv. Where appropriate, a projection sign shall be integrated with the entrance feature.
- v. Where appropriate, the width of the entrance feature may be seamlessly continued to adjacent buildings.
- vi. Hard landscaping shall be contiguous and seamlessly integrated with the public sidewalk with no grade adjustments.
- vii. Where any structure encroaches on the public realm all maintenance and upkeep of the structure and surrounding area is the responsibility of the private owner.

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

3.4.9 Landscaping & Screening Standards

| | |
|--------------------------|---|
| a) Development Standards | All landscaping shall comply with the <i>General Design Standards</i> except where Frontage Type Standards in Section 3.4.8 take precedence. Where possible use plants with seasonal interest. Drought tolerant plants are encouraged. |
| b) Mandatory Requirement | Any portion of a site not occupied by a structure , parking area, patio , walkway, or storage area shall be landscaped. |
| c) Number of Trees | All lots greater than 2,500 m ² shall be required to provide a minimum of 1 tree per 35 m ² , based on 10% of the site , where 40% of those trees shall be coniferous, except for municipal reserve land . Unless otherwise provided for by a frontage standard, all lots less than 2,500 m ² shall be required to provide a minimum of 3 trees, except for municipal reserve land . |
| d) Tree Size | At the time of planting each coniferous tree shall be at least 2.0 m in height and each deciduous tree shall have a caliper of at least 50 mm. |
| e) Tree Location | Where tree requirements are specified in the Frontage Type Standards, as per Section 3.4.8, the total number of trees required for the remainder of the lot shall be subtracted by those trees to be located in the principal frontage . |
| f) Number of Shrubs | All lots greater than 2,500 m ² , shall be required to provide a minimum of 1 shrub per 35 m ² , based on 10% of the site , except for municipal reserve land . Unless otherwise provided for by a frontage standard, all lots less than 2,500 m ² shall be required to provide a minimum of 5 shrubs, except for municipal reserve land . |
| g) Shrub Size | At the time of planting each shrub shall be at least 300 mm deciduous height or 450 mm coniferous spread. |
| h) Soil Requirements | A minimum of 15 cm of high quality soil and growing material is required for all planting areas. |
| i) Fencing / Screening | A fence , wall, or screening may not exceed 1.0 m within a principal frontage or 1.8 m in height on any other portion of the lot . A permit is |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

required for a [fence](#) exceeding 1.0 m in height on a [secondary frontage](#).

3.4.10 Parking, Access & Loading Standards

a) MINIMUM PARKING STANDARDS

| | | | |
|------|----------------------------------|--|---|
| i. | Agriculture Uses | Agriculture - General | - |
| | | Agriculture - Intensive | - |
| | | Agriculture - Urban | - |
| | | Cannabis Production and Processing | - |
| | | Medical Cannabis Production | - |
| ii. | Residential Uses | Dwelling Unit(s) | |
| | | Mobile Home | 1 stall per unit |
| | | Temporary Dwelling Unit(s) | |
| iii. | Lodging Uses | Bed & Breakfast | 1 stall per unit or bedroom |
| | | Campground | - |
| | | Hotel / Motel | 1 stall per unit or bedroom |
| iv. | Business Uses | Arts & Crafts | 1 stall per business |
| | | Home Based Business - Major | |
| | | Home Based Business - Minor | - |
| | | Office | 1 stall per 100 m ² of lot coverage |
| v. | Commercial Uses | Adult Entertainment | |
| | | Drive Through Facility | |
| | | Entertainment Establishment | 1 stall per 100 m ² of lot coverage |
| | | Gas Station | |
| | | Golf Course | |
| | | Kennel | |
| | | Restaurant / Café | 2 stalls per 100 m ² of lot coverage |
| | | Restricted Substance Retail | 1 stall per 100 m ² of lot coverage |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

| a) MINIMUM PARKING STANDARDS | | |
|------------------------------|--|---|
| | Retail & Service - General | |
| | Retail & Service - Large | |
| | Show Home | - |

| b) MINIMUM PARKING STANDARDS | | | | | |
|---|------------------------------------|--|---|---|---|
| vi. | Industrial Uses | Industrial - Medium | | | |
| | | Industrial - Light | 1 stall per 100 m ² of lot coverage | | |
| | | Recreational Vehicle Storage | | | |
| | | Wash Station | | | |
| vii. | Institutional Uses | After Life Care | | | |
| | | Cemetery | | | |
| | | Culture | | | |
| | | Education | 2 stalls per 100 m ² of lot coverage | | |
| | | Government | | | |
| | | Hospital | | | |
| | | Human Services | | | |
| | | Recreation - Active | | | |
| | | Recreation - Passive | - | | |
| | | Parking Lot with no associated Use | - | | |
| | | Special Events | - | | |
| | | viii. | Other Uses | Accessory Building or Structure | - |
| | | | | Public Utility | - |
| Excavation, Stripping & Grading | - | | | | |
| Private Utility | - | | | | |
| Sign | - | | | | |
| Temporary Development | - | | | | |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

c) PARKING STANDARDS

| | | |
|------|----------------------------------|---|
| i. | Development Standards | Any parking area having four or more parking stalls that are visible from an adjoining site , or from a thoroughfare other than a lane , shall have perimeter planting. The location, length, thickness and height of such perimeter planting at maturity shall, in conjunction with a change in grade or other natural or man-made features, be sufficient to provide substantial interruption of the view of the parking area from any adjoining site and enhance the view of the parking area from any adjacent thoroughfare . |
| ii. | Pedestrian Network Design | Parking lots shall be designed to efficiently, comfortably, and to safely direct pedestrians from parking areas and entrance features . Walking areas shall be a minimum of 2 m wide, be well marked, be separated by grade from driving or parking areas, and be integrated with landscaping. Parking lots shall not be located in the principle frontage . |
| iii. | Parking Location | All parking stalls shall be provided on-site except where, at the discretion of the Development Authority , street parking may be accommodated. Street parking can be considered where the curb frontage is a minimum of 6.m per stall excluding access locations and appropriate safety distances. Where contiguous curb frontage space is available, street parking may be considered part of the minimum parking requirements as per Section 3.4.10 (a). |
| iv. | Shared Parking | Where multiple buildings or uses are located on a site , parking minimums in Section 3.4.10 (a) may be reduced to the satisfaction of the Municipality . |
| v. | Parking Lot Size | A maximum of 50% of the site can be used to accommodate parking. Where parking requirements in Section 3.4.10 (a) exceed this amount, a strategy for reducing the parking need shall be provided. |
| vi. | Accessible Vehicle Parking | For all lots with 11 or more vehicle parking stalls , accessible parking shall be provided in a location with the easiest pedestrian access to the principal building entrance and shall be provided in accordance with the <i>Barrier Free Design Guide</i> as per the Alberta Safety Codes Council . |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

c) PARKING STANDARDS

| | | |
|-------|---|--|
| vii. | Parking Structure Design | Accesses shall be designed as an integral part of the building façade so they are in scale and character with the rest of the building . Accesses shall not interfere with pedestrian movement. |
| viii. | Parking Lot Stall Location | Parking stalls shall be spread out through the site and integrated with buildings and landscaping to provide a comfortable pedestrian network. |
| ix. | Landscaped Islands | In addition to the landscape requirements in Section 3.4.9 in no case shall there be less than 1 tree and 3 shrubs per landscaped island. |
| x. | Calculations | Where a fractional figure occurs, the requirement shall be rounded up to the next whole number. |

d) BICYCLE PARKING STANDARDS

| | | |
|------|--|---|
| i. | Development Standards | Bicycle parking structures shall be highly visible and shall include a permanent rack or hook-up system. Creative integration with the development is encouraged. |
| ii. | Minimum Bicycle Parking | All non-residential buildings less than 4,600 m ² shall provide parking for at least 6 bicycles per building . All non-residential building greater than or equal to 4,600 m ² shall provide parking for 6 bicycles per entrance. |
| iii. | Bicycle Parking Location | All bicycle parking structures shall be located within 10 m of a public entrance, but shall not impede pedestrian circulation or access to a building . |
| iv. | Bicycle Parking Access | Where a change in grade occurs in the bicycle parking network, ramps or similar structures shall be used to access all bicycle parking structures . |

PART 3: LAND USE DISTRICTS

INTEGRATED NEIGHBOURHOOD DISTRICT (IN)

e) ACCESS STANDARDS

| | | |
|------|-----------------------------|---|
| i. | Number of Accesses | All lots require a minimum of 1 access to the site from a legal and physical public roadway and shall be approved by the Municipality . Additional accesses shall have prior approval from the Development Authority . |
| ii. | Lane Access | Lots fronting an arterial roadway shall be required to have access from a rear lane . Where the site is adjacent to a lane , the lane will be used for all vehicular access unless otherwise authorized by the Municipality , such as where a front attached garage frontage type is used with a lane . |
| iii. | Shared Access | Shared access between 2 or more adjacent lots may be considered to provide more effective access arrangements, to reduce curb cuts, and / or to reduce any negative impact on the public realm . |

f) LOADING STANDARDS

| | | |
|------|------------------------------------|--|
| i. | Development Standards | All non-residential uses shall provide sufficient space and access for loading vehicles to the satisfaction of the Development Authority . |
| ii. | Clearance | All loading areas shall provide a minimum of 5.3 m vertical clearance from grade . |
| iii. | Loading Space Size | All loading space shall be at least 4 m wide and 8 m long. |
| iv. | Access | Access shall be from a public road, a lane , or a clearly defined traffic aisle, and shall not obstruct patron / emergency vehicle circulation. |
| v. | Location | Loading areas shall be located to the side or rear of a lot . |

PART 5: ADMINISTRATION

GENERAL PROVISIONS

5.28 CERTIFICATE OF COMPLIANCE

- 5.28.1 Upon application, the [Development Authority](#) may endorse a [certificate of compliance](#) provided that:
- a) The [real property report](#) is prepared by a registered Alberta Land Surveyor and all costs associated with the [certificate of compliance](#) have been paid;
 - b) The development on the property is in full compliance with the provisions of [Our Zoning Blueprint](#);
 - c) The [applicant](#) has applied for and received a [development permit](#) granting a [variance](#) of one or more provisions of [Our Zoning Blueprint](#);
 - d) Approval of a [subdivision](#) has resulted in a [variance](#) of one or more provisions of [Our Zoning Blueprint](#); and / or
 - e) A development is considered an existing [non-conforming building](#) or [use](#) as a result of the adoption of [Our Zoning Blueprint](#).
- 5.28.2 The [Development Authority](#) shall not be liable for any damages arising from the [use](#) of a letter respecting compliance containing errors where the errors are the result of incorrect or incomplete information on the [real property report](#).
- 5.28.3 The [Development Authority](#) may only endorse a [certificate of compliance](#) which is attached to, or forms part of, a [real property report](#) that is no more than 5 years old.

5.29 COMMUNICATION FACILITIES

- 5.29.1 Industry Canada is responsible for regulating telecommunications facilities in Canada. The participation of the [Municipality](#) in the approval process is part of the consultation process mandated by Industry Canada and is not a delegation of a federal decision-making authority, nor does it confer a right on the [Municipality](#) to refuse the location of a communication facility.
- 5.29.2 As part of the consultation, the [Municipality](#) may request the following:
- a) Mitigation of the visual effect of the antenna and supporting [structure](#) through the integration of [soft landscaping](#);
 - b) Protective fencing around the [structure](#); or
 - c) Limitations to illumination and advertising unless required by Transport Canada [regulations](#), and except for a manufacturer's logo.

PART 5: ADMINISTRATION

GENERAL PROVISIONS

5.30 DEMOLITION OF BUILDINGS

- 5.30.1 All pre-conditions, as determined by the [Municipality](#), for a demolition permit must be satisfied prior to an application being made.
- 5.30.2 Where a [development permit](#) is to be approved for the demolition of a [building](#), the [Development Authority](#) may require the [applicant](#) to provide a performance bond to cover costs of reclamation and damage to public and quasi-public utilities, public roadways and sidewalks, and to carry sufficient comprehensive liability insurance naming the [Municipality](#) as an insured party in all public liability policies.
- 5.30.3 The [applicant](#) shall keep sidewalks and public road clear of spilled dirt and debris.
- 5.30.4 Provisions shall be made at all times for the safe [passage](#) of both pedestrian and vehicular traffic around the property. Material or equipment shall not be placed on any public roadway or other public property that will hinder the [passage](#) of pedestrians and vehicles except as authorized by the [Municipality](#).
- 5.30.5 The [applicant](#) shall agree to indemnify and save the [Municipality](#) harmless from all costs, expenses, and charges of, and incidental to, the repair and replacement of sidewalks, curbs, gutters, land and other surface utilities, and improvements [adjacent](#) to the [site](#) and necessitated by damage done by the [applicant](#), its agents or employees in the demolition of this property.
- 5.30.6 It is the responsibility of the [applicant](#) to apply for a final inspection after the work is complete. All sidewalks, curbs, gutters, Lanes and other surface infrastructure must be completely clear of snow, soil, mud or other debris prior to the inspection being performed.
- 5.30.7 The [applicant](#) shall provide fencing around open basements or excavations to protect the general public.
- 5.30.8 The [applicant](#) shall level the basement area if new construction does not start immediately after removal of the [building](#).
- 5.30.9 The [applicant](#) shall remove debris from the [site](#). The [applicant](#) shall excavate down and abandon existing water, wastewater and storm services at point of connection to the main lines, and remove existing curb cock box and rod; or abandon at property line depending on future plans for the [site](#).
- 5.30.10 It is the responsibility of the [applicant](#) to advise the [Municipality](#) within 2 business days before the date of commencement of the demolition or moving of the [building](#) and [structures](#) from the [site](#). If there is damage to the sidewalks, curbs, gutters and other surface infrastructure after this date, the repair will be the responsibility of the owner.

PART 5: ADMINISTRATION

GENERAL PROVISIONS

5.31 LANDSCAPING COMPLIANCE

- 5.31.1 Except for the planting of a lawn, no hard or [soft landscaping](#) shall be permitted near a curb cock, fire hydrant, or utility pedestal.
- 5.31.2 All development shall be graded in accordance with the *Surface Drainage Bylaw 732-08*, as amended. With regard to grading a [Lot](#), no slope shall be designed that is less than 2% or more than 20%. Where slopes exceed 20%, a retaining wall shall be required. Where a retaining wall exceeds 1.0 metre in height, an engineer's stamped drawing, including a proposed [lot](#) drainage plan, shall be submitted.
- 5.31.3 Every owner or occupant of land shall be responsible for maintaining and controlling the weeds on public property [adjacent](#) to said [lot](#) except [reserve land](#).
- 5.31.4 Any person removing or causing to be removed from public property any hard or [soft landscaping](#) or [structure](#) without authorization from the [Development Authority](#), may be, at the discretion of the [Development Authority](#), liable for the cost of replacing the hard or [soft landscaping](#) or [structure](#) and associated works necessary to restore the public property to its original condition.
- 5.31.5 Where a tree or shrub planted on private property is broken, uprooted or encroaches onto public property, the owner of the tree or shrub shall be liable for the removal of the broken, uprooted or encroaching material from the public property.

5.32 DIRECT CONTROL GENERAL PROVISIONS

- 5.32.1 The purpose of a direct control district is to provide for development(s) that, because of its unique characteristics, innovative ideas or unusual [site](#) constraints require specific regulation unavailable in other [land use districts](#).

PART 5: ADMINISTRATION

GENERAL PROVISIONS

- 5.32.2 An [applicant](#) for a direct control district must provide a written statement indicating why, in the [applicant's](#) opinion, a direct control district is necessary and why the same result cannot be achieved through the [use](#) of a [land use district](#) in [Our Zoning Blueprint](#).
- 5.32.3 In evaluating an application for a direct control district, [Council](#) shall take into account the objectives and policies contained in [Our Complete Community](#) and any statutory plans that relate to the area.
- 5.32.4 [Council](#) shall establish appropriate development standards in the bylaw approving a direct control district and shall also identify whether [Council](#) or the Chief Administrative Officer will be the [Development Authority](#) to decide on subsequent [development permit](#) applications.
- 5.32.5 In a direct control district where [Council](#) is the [Development Authority](#), decisions on minor [development permit](#) applications are hereby delegated to the Chief Administrative Officer or his / her delegate. For the purposes of this Section, minor [development permits](#) shall be:
- a) Signs;
 - b) Home based businesses; and
 - c) Changes of [use](#) that comply with the direct control bylaw and require no [variance](#).
- 5.32.6 Where a [site](#) is designated a direct control district:
- a) Pursuant to [Our Zoning Blueprint](#), a reference to a Section of [Our Zoning Blueprint](#) within the direct control bylaw is deemed to be a reference to the Section as amended from time to time, unless a contrary intent is stated in the direct control bylaw;
 - b) Pursuant to a previous [land use bylaw](#) and where such designation is continued pursuant to [Our Zoning Blueprint](#), the direct control bylaw, as approved by [Council](#) at the time such designation was made, will continue to apply, unless a contrary intent is set out in the bylaw designating the direct control.

PART 6: DEFINITIONS

B

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| Balcony | a horizontal platform that is attached to a building above the first storey level and is intended for use as an outdoor amenity space. Balconies shall adhere to all setbacks unless otherwise stated in a land use district . |
| Beaumont Urban Design Guidelines | the design guidelines adopted by Beaumont Council , as may be amended or replaced from time to time. |
| Bed & Breakfast | a quasi-residential use of a dwelling unit, whereby remuneration is provided in exchange for temporary food service and lodging. A bed and breakfast use shall not include self-contained dwelling units or amenities for extended domestic use . |
| Bicycle Parking | a rack, railing, locker, or other structurally sound device which is designed for the securing of one or more bicycles in an orderly fashion. |
| Blank Wall | an exterior wall of a building containing no articulation, change of colour, or openings within it. |
| Block | all contiguous lots, passages , and lanes bounded by thoroughfares, rights-of-way, or parks. |
| Block Length | the combined and contiguous frontages of one side of a block , as measured at the front lot line , bound by, but does not include thoroughfares, pedestrian or vehicular rights-of-way, private roads, public utility lots and / or parks. Access(es) without a road or pathway is part of the block length measurement, however, side lot lines are not included. |
| Buffer | a row of trees or shrubs, an earth berm, or fence to provide screening, noise abatement and separation between lots, land use districts and non-compatible uses . |
| Building | anything 10 square metres or greater constructed or placed on, in, over or under land but does not include a highway or public roadway or a bridge forming part of a highway or public roadway. |
| Building Height | the height of a building as measured in storeys up to the eave and does not include the roof, attic, walk-out basement, or other accessory structure . |

PART 6: DEFINITIONS

D

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| Day Home | a child care facility operated from a private residence and complies with the <i>Alberta Family Day Home Standards</i> but does not include child care programs as defined by the <i>Child Care Licensing Act</i> . |
| Deck | an uncovered structure that is raised more than 0.6 metres above ground level at any point, with or without stairs and intended for use as a private outdoor amenity space. A deck shall meet all required setbacks for the associated building . |
| Density | the number of dwelling units allowed for each lot either by a total number of dwelling units or by the number of dwelling units per hectare of land not including those lands within the lot required for land required for public utilities, environmental reserve or municipal and school reserve. |
| Development | <ol style="list-style-type: none">an excavation or stockpile and the creation of either of them; ora building or an addition to or replacement or repair of a building and the construction or placing of any of them in, on, over or under land; ora change of use of land or a building or an act done in relation to land or a building that results in or is likely to result in a change in the use of the land or building; ora change in the intensity of use of land or a building or an act done in relation to land or a building that results in, or is likely to result in, a change in the intensity of use of the land or building. |
| Development Agreement | a legal agreement between the applicant and the Municipality committing to the provision of utility and infrastructure services and any other matter required by a condition of the development permit or subdivision approval. |
| Development Authority | the official or officials of the Municipality with the responsibility of receiving, considering, and deciding on applications for development under Our Zoning Blueprint and for enforcing the provisions of Our Zoning Blueprint . |
| Development Permit | a document that is issued under Our Zoning Blueprint and authorizes a development. A development permit is separate and distinct from a building permit . |

PART 6: DEFINITIONS

D

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| Discretionary Use | those uses of land or buildings provided for in the land use district Schedule of <i>Our Zoning Blueprint</i> for which a development permit may be issued with or without conditions. |
| Drive Through Facility | a development which serves customers travelling in motor vehicles driven onto the lot where such business is carried on, where normally the customer either remains in the vehicle for service, or parks the vehicle for a short period for the purpose of doing business at the premises. Drive through facilities include, but are not limited to, drive-through food services and drive-through banks. |
| Dwelling Unit(s) | a self-contained living premise with cooking, eating, living, sleeping and sanitary facilities for domestic use by one or more individuals. |

PART 6: DEFINITIONS

E

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| Eave | the junction of a building wall and an overhanging roof. |
| Education | development of public and private places of learning for any age including licensed child care facilities and / or research facilities. |
| Emergency Service Training Facility | development used to support specialized protective and emergency service training, including indoor firing range and protective services exercises and simulations. Does not include a police station, fire station, ambulance station, or outdoor firing range and does not include training facilities required for the standard training and development of local public service agencies. |
| Entertainment Establishment | development for spectator or participatory uses , both indoor and outdoor, including, but not limited to, billiards, arcades, bowling alleys, theatres, movie theatres, fairgrounds, etc. |
| Entrance Feature | the primary entrance to a building and the surrounding articulation that makes the entrance visible and gives it character, including but not limited to, porches, windows, projection over the entrance and stoops, but does not include stairs or accessibility features . |
| Equestrian Facility | a facility used for the training of horses and riders and may include facilities for horse boarding and grooming, horse shows and equestrian competitions. |
| Excavation, Stripping & Grading | the physical alteration of the land typically for the purpose of construction. Changes to drainage patterns are included in this use ; however, normal agricultural activity is excluded from this use , including but not limited to farm cultivation, and grading activities for regular maintenance. |

PART 6: DEFINITIONS

F

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| Façade | the exterior wall of a building . |
| Fence | a vertical physical structure constructed to provide screening, sound abatement, or to enclose a lot . |
| Front Lot Line | the lot line that is adjacent to a principal thoroughfare . |
| Frontage | the area of a lot between a building façade and a curb inclusive of its built and landscape components. |

PART 6: DEFINITIONS

L

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| Land Titles Office | the office responsible for registering land ownership rights in Alberta. The <i>Land Titles Act</i> provides the legislative framework for the department to register land related documents that both create and terminate legal rights in property. |
| Land Use Bylaw | a bylaw of the Municipality passed by Council pursuant to the provisions of the <i>Municipal Government Act</i> which regulates the development and use of land and buildings within the Municipality . |
| Land Use District | an area of land designated under Part 3 of Our Zoning Blueprint , for which a specific set of building standards and Uses have been set forth in Our Zoning Blueprint or, in the case of a direct control district, are determined by Council . |
| Landscaped Area | the area of a lot planted or to be planted with trees, shrubs or other vegetation including the soil or bedding material areas associated with plantings. A landscaped area does not include the footprint of a building , decks , patio , sidewalk, driveway, parking lot , or other similar hard landscaping . |
| Landscaping, Hard | those materials used in the landscaping design for a lot or building that are non-living and include, but are not limited to, paving material, wooden timbers, concrete products, manufactured playground equipment, and streetscape furniture. |
| Landscaping, Soft | to preserve or change the natural features of a lot by adding lawn, trees, shrubs, ornamental plantings, and materials as used in modern landscape architecture. |
| Lane | a secondary vehicular access located to the side or rear of a lot and provides access to service areas, parking, accessory structures , and / or containing utility easements. |
| Loading Space | an area to accommodate a vehicle while being loaded or unloaded. |

PART 6: DEFINITIONS

L

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| Lot | <ul style="list-style-type: none">a) a quarter section;b) a river lot or settlement lot shown on an official plan as defined in the <i>Surveys Act</i> that is filed or lodged in a land titles office;c) a part of a parcel where the boundaries of the part are separately described in a certificate of title other than by reference to a plan of subdivision; ord) a part of a parcel where the boundaries of the part are described in a certificate of title by reference to a plan of subdivision. |
| Lot Area | the area contained within the boundaries of a lot as shown in a plan of subdivision or described in a certificate of title . |
| Lot Coverage | the portion of the lot area covered by all covered structures . |
| Lot Line | the boundary that legally and geometrically demarcates a lot , also known as a property line. |
| Lot Width | the perpendicular horizontal distance between the side boundaries of the lot, measured at the front property line, or in the case of an irregular shaped lot , as measured at 6 metres from the front lot line |

PART 6: DEFINITIONS

P

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| Parking Lot | a parking area which is located on an area of land and is not accessory to a particular use or development. |
| Parking Stall | a space set aside for the parking of one, or any, vehicle, which within urban areas of the City of Beaumont must be hard-surfaced to the satisfaction of the Development Authority. |
| Passage | a pedestrian connector passing between or through a building , providing shortcuts through long blocks, and / or sometimes connecting rear parking areas with frontages . |
| Patio | any developed surface which is less than 0.60 metres above ground level and is adjacent to a building . |
| Pedestrian Circulation Plan | map(s) and / or plans that demonstrate where and how pedestrian use the site . Requirements include identification of locations, dimensions, and materials of sidewalks, pathways, crossings and similar infrastructure that pedestrians will use . Can include additional design characteristics such as winter design or place making elements. |
| Pergola | a shade shelter that is either attached to the principal building or free-standing with pillars that support cross-beams forming a part of an open-beam roof. |
| Permitted Use | the use of land or building provided for in the land use district of <i>Our Zoning Blueprint</i> for which a development permit shall be issued with or without conditions upon an application having been made which conforms to <i>Our Zoning Blueprint</i> . |
| Planned Development | an area subject to an approved area structure plan or requiring an approved area structure plan prior to urban-style development. |
| Plan of Subdivision | a plan of survey prepared in accordance with the <i>Land Titles Act</i> for the purpose of effecting a subdivision . |
| Porch | an entrance structure typically attached to the front or sides of a building at the ground floor entry level, often consisting of a roof and floor, where the front and sides of the structure may be enclosed. |

PART 6: DEFINITIONS

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| Principal | the main use or structure on a lot that occupies the major or central portion of a lot and constitutes the principal purpose for which the site is used. Principal uses may be located within a structure , or portion of a structure that is separated structurally from other uses within the same structure . One or more principal uses may occur on a lot . |
| Principal Thoroughfare | the thoroughfare clearly associated with the front façade of principal buildings along a block face. In the case of corner lots , the Development Authority may determine that all thoroughfares, except for one or a lane , are a principal thoroughfare , if such a determination is in the interest of protecting the quality of the public realm . If the position of the principal thoroughfare in relation to the corner lot is unclear, the Development Authority will make a determination on this matter, taking into account the following factors: <ol style="list-style-type: none">The orientation of principal buildings at the intersection, where principal buildings at the intersection are oriented toward one of the thoroughfares, this thoroughfare will generally be considered the principal thoroughfare.The lot width. Among the lot lines adjacent to the thoroughfares, the narrowest lot width will generally be associated with the principal thoroughfare.The width of the thoroughfares. The thoroughfare with the widest pedestrian space will generally be considered the principal thoroughfare.Pedestrian counts. The thoroughfare with the highest weekday, peak-hour pedestrian counts will generally be considered the principal thoroughfare. |
| Principal Frontage | the area adjacent to the principal thoroughfare from the curb to the building façade and includes the public frontage and private frontage . For a corner lot , the principal frontage includes the corner area of the lot by extending the building façade line to the secondary thoroughfare . |
| Private Frontage | the area between the building façade and the lot line , and applies to the principal frontage and secondary frontage . |

PART 6: DEFINITIONS

P

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| Private Utility | systems and facilities associated with water, wastewater, storm, power, heating and cooling, energy, waste, transportation, and telecommunications for individual use . |
| Projection | any horizontal structural or architectural element, building feature or other object that juts out, overhangs, or protrudes into the prescribed setback and is located above the foundation. |
| Public Frontage | the area adjacent to a thoroughfare between the curb and the front lot line , and applies to the principal frontage and secondary frontage . |
| Public Realm | all exterior places, linkages, and built form elements that are physically accessible regardless of ownership. The public realm includes, but is not limited to, rights-of-way, parks, squares, plazas, courtyards, paths, boulevards, and sidewalks. |
| Public Utility | systems and facilities associated with water, wastewater and storm, power, heating and cooling, energy, solid waste, transportation, and telecommunications for public or collective use . |

PART 6: DEFINITIONS

R

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| Real Property Report | a codified standard adopted by the Alberta Land Surveyor's Association and indicates the location of buildings and / or structures on a lot , certified by an Alberta Land Surveyor duly signed. |
| Rear Lot Line | a lot line that is opposite the front lot line and is not adjacent to a thoroughfare . |
| Recreation – Active | development for leisure activities requiring indoor and / or outdoor facilities, including but not limited to, parks, swimming pools, gyms, field sports, arenas, or batting cages. |
| Recreation - Passive | leisure activities that require little to no alteration or formal development of a lot for public or private enjoyment. |
| Recreational Vehicle | a portable structure designed and built to be carried on a vehicle, or a unit designed and built to be transported on its own wheels, to provide temporary living accommodation for travel and recreational purposes and/or motorized sports activities conducted outdoors on land and/or water and include, but are not limited to, such vehicles as a motor home, camper, travel trailer, tent trailer, motor bikes, snowmobiles, boats and utility trailers, but does not include a manufactured or mobile home . |
| Recreational Vehicle Storage | development of a site used for the business of storing and parking of recreational vehicles. |
| Reserve Land | municipal, school, municipal/school or environmental reserve that has been dedicated in accordance with the Municipal Government Act . |
| Restaurant / Café | development where prepared food and beverages are offered for sale to the public for consumption on-premises or off-premises. |
| Restricted Substance Retail | a retail establishment licensed under the Alberta Gaming and Liquor Commission for the sale of liquor or cannabis for consumption off-premises, and may include cannabis accessory . Where cannabis is sold, no restricted substance retail store shall be located within 200 m from any other restricted substance retail store selling cannabis , as measured from the closest point of an exterior wall of each restricted substance retail store. |

PART 6: DEFINITIONS

R

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| Retail & Service – General | a development that provides goods or services directly for sale to the consumer, and where such goods or services are available for purchase on the premises and can include cannabis accessory . Outdoor display that does not hinder pedestrian movement may be permitted. |
| Retail & Service - Large | a development, often with outdoor display, which provides goods or services for consumer purchase or rental and includes but is not limited to vendors of lumber and building supplies, landscaping supplies and equipment, vehicles, watercraft, and/or outdoor structures as prefabricated sheds, decks , patios, swimming pools and play equipment. Outdoor displays that are accessory to a principal use may occur providing the scale of such activities does not unduly conflict with the primary purpose of or dominate the use of the site . Seasonal displays or seasonal outdoor retail may be permitted. |
| Right-of-Way | the total area of land reserved or dedicated as a thoroughfare , lane , pedestrian way, or utility line. |

PART 6: DEFINITIONS

S

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| Secondary Thoroughfare | a thoroughfare that is not a principal thoroughfare , and may include a lane , at the discretion of the Development Authority . |
| Secondary Frontage | the area adjacent to the secondary thoroughfare from the curb to the building façade and includes the public frontage and private frontage , but excludes the principal frontage . |
| Security | an irrevocable and continuous letter of credit issued by a chartered bank or treasury branch, or other security provided that such security shall be in terms and form to be approved by the Municipality and its solicitors. |
| Setback | the minimum distance between the foundation wall of a building or structure and a lot line . |
| Shadow Plan | a conceptual lot layout and multi-modal transportation layout for future phases of subdivision identified in the appropriate area structure plan , neighbourhood structure plan and / or outline plan. |
| Show Home | a permanent dwelling unit that is constructed for the temporary purpose of illustrating to the public the type or character of a dwelling or dwellings to be constructed in other parts of a subdivision or development area. Show homes may contain offices for the sale of other lots or dwellings in the area. A show home may only remain in use for a period of 2 years or less. A show home may not be occupied by any person for the purpose of residency. |
| Side Lot Line | the lot line other than the front lot line or rear lot line and can be adjacent to the secondary thoroughfare . |
| Sign | a visual medium used to convey information by way of words, pictures, images, graphics, emblems or symbols, or any device used for the purpose of providing direction, identification, advertisement, business promotion, or the promotion of a person, product, activity, service, event, or idea. |
| Site | an area of land on which a building or use exists or for which an application for a development permit is made. |
| Site Area | the gross area of a site . |

PART 6: DEFINITIONS

U

Use

the utilization of land for a particular development activity.

PART 6: DEFINITIONS

V

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| Variance | an alteration or change to a standard prescribed by Our Zoning Blueprint that is authorized by the Subdivision Authority , the Development Authority , or the Subdivision and Development Appeal Board . |
| Violation Tag | the document issued by the Municipality to a person who has committed an offence under Part 5 of Our Zoning Blueprint . |

Planning & Development
5600 - 49 Street
Beaumont, AB T4X 1A1
780-929-8782
development@beaumont.ab.ca

June 25, 2025

DATE RECEIVED
OFFICE USE ONLY

July 16, 2025

DATE PAID
OFFICE USE ONLY

Note:
Building, Electrical, Plumbing, and Gas Permits each have their own application forms.

Property Information

Street Address: 9 COLONIALE WAY, BEAUMONT, AB
Plan: 912 1656 Block: 103 Lot: 1

Applicant and Property Owner Information

Applicant/Contractor Name: SABRINA KEICHINGER / MILLBREE ARCHITECT ART AND DESIGN
Mailing Address: [REDACTED]
Town: EDMONTON Postal Code: T5M 0T4
Phone: [REDACTED] Cell Phone: [REDACTED]
Email (required): [REDACTED]

Is the Applicant also the Registered Owner? Yes (Do not fill out below) No (Fill out below - written authorization from registered owner required)

Owner Name: RAGHUBIR BHANDAL, KULWINDER TOOR Per Purchase Agreement
Mailing Address: 301 - 5007 - 50 STREET
Town: BEAUMONT Postal Code: T4X 1J9
Phone: [REDACTED] Cell Phone: [REDACTED]
Email (required): [REDACTED]

Proposed Development: [REDACTED] Square Footage (m²): 2727 FOOTPRINT/4983 FULL

Check one of the following:
 Commercial
 Industrial
 Institutional
 Other Non-Residential Use: Integrated Neighbourhood District
 Signage → Provide Construction Value: _____
(approx. cost of material and labor)

Has work on the above indicated item already commenced? Yes No

Description of Work and Land Usage:

2 two-storey Commercial (CRU) buildings (shell only) to be constructed on the above described site.

Proposed uses vary. East building proposes daycare use on upper floor with retail and service - general + restaurant/cafe uses below.

West building proposes office use on upper floor with retail and service - general + restaurant/cafe uses below.

Applicant Authorization

- I am the owner/agent with the consent and authority of the owner that is the subject matter of this permit application.
- I hereby give my consent to allow any authorized person pursuant to the Municipal Government Act Section 542 the right to enter the land and/or building(s) with respect to this application only.
- I understand this is only an application and does not constitute approval to commence construction.
- I declare that the information contained in this application is correct and true to the best of my knowledge. I agree
- I declare that I will notify the Development Authority of any proposed changes to the plans submitted with this application.
- I consent to receiving notifications & correspondence regarding this application via email to the address provided on this application.

Applicant Signature: [REDACTED] Date: JUNE 23, 2025

OFFICE USE ONLY

Permit Number: **2025-077**

Building Permit Number: _____

Mail Pick-up

Authorization or ID Received

Land Use District: **IN**

Subdivision: **COLONIALE**

Tax Roll: **001724**

Permitted Use

Permitted Use w/ Variance

Discretionary Use

Fee: _____ Receipt #: **335784**

Development Permit: **6,000.00**

Variance: _____

Notification Fee: _____

Other: _____

Total Fees: 6,000.00

OFFICE USE ONLY

Development Permit

Date Deemed Complete: _____ Date of Decision: _____
(See attached Notice of Decision)



DEVELOPMENT / BUILDING PERMIT AUTHORIZATION FORM

OWNER INFORMATION

I (We), RAGHUBIR (GARRY) BHANDAL

(name(s) of Registered Land Owner(s))

being the registered land owners of:

Municipal Address: 9 COLONIALE WAY, BEAUMONT, AB Postal code T4X 1M2

Legal Description: Plan 912 1656, Block 103, Lot 1

Owner Phone number: [REDACTED]

Owner Email: [REDACTED]

Do hereby authorize:

APPLICANT INFORMATION

Company: Millbree Architect Art and Design

Contact Name: Sabrina Keichinger

Address: [REDACTED] Postal code [REDACTED]

Phone: [REDACTED]

Email: [REDACTED]

to make application for the necessary building / development permits required to complete the following project: 9 Coloniale Way - Belleview Centre

For Commercial Only:

Owner is responsible for all costs associated with water metering changes, including all piping, and removal and/or replacement of water meter(s). Changes are required to water meter (please circle) **Yes** **No**

If yes, please contact Public Works at 780-929-4300.

ALL REPORTS WILL BE PROVIDED TO OWNER AND APPLICANT

RAGHUBIR BHANDAL
(Print name of Registered Land Owner)

(Print name of Registered Land Owner)

(Signature of Registered Land Owner)

(Signature of Registered Land Owner)

Date

23 JUNE 2025

Date

The personal information requested on this form is being collected under the authority of Section 33(c) of the Freedom of Information and Protection of Privacy (FOIP) Act. The information collected will be used for the purpose of permit review and inspection processes and may be communicated to relevant City Business Units, utility providers, and Alberta Health Services. It may also be used to conduct ongoing evaluations of services received from City Business Units. The name of the applicant and the nature of the permit will be available to the public. If you have any questions about the collection or use of your personal information, contact the City of Beaumont's FOIP Coordinator at 5600-49th Street, Beaumont, Alberta, T4X 1A1 or 780.929.8782.



City of Beaumont
5600 - 49 Street
Beaumont, Alberta T4X 1A1
Phone: (780) 929-8782
Fax: (780) 929-3300
Email: development@beaumont.ab.ca

DECLARATION - ABSENCE OF ABANDONED WELLS ON PARCEL

I, SABRINA KEICHINGER / MILLBREE ARCHITECT ART + DESIGN, have reviewed information from the Energy Resource and Conservation Board ("ERCB") and can advise that the information shows the absence of any abandoned wells on the parcel that is the subject of the attached application for Development Permit.

Subject Parcel:

Lot: 1 Block: 103 Plan: 912 1656

Municipal Address: 9 Coloniale Way, Beaumont, AB



Signature

SABRINA KEICHINGER

Printed Name

June 19, 2025

Date

SEE REVERSE FOR INFORMATION



City of Beaumont
 5600 - 49 Street
 Beaumont, Alberta T4X 1A1
 Phone: (780) 929-8782
 Fax: (780) 929-3300
 Email: development@beaumont.ab.ca

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Millbree Architect Art and Design c/o Sabrina Keichinger
 [REDACTED]
 [REDACTED]

Proposed Development: Two 2-storey Commercial (CRU) Buildings – East Building includes Daycare on upper floor with retail - service general and restaurant/café uses below.

Legal Description: Plan 912 1656, Block 103, Lot 1

Municipal Address: 9 Coloniale Way, Beaumont, AB

Land Use District: Integrated Neighbourhood

Permit Application No: 2025-077

Tax Roll: 001724

Development Permit Status: Approved with conditions

Development Permit Authorization

This Development Permit authorizes the construction of two (2) two-storey buildings, with a total floor area of 4,993 square metres (East Building: 2,152 square metres; West Building: 2,841 square metres), a total building footprint of 2,727 square metres, and associated site improvements, in accordance with the stamped approved drawings and the provisions of the City of Beaumont Land Use Bylaw 944-19.

This Development Permit further authorizes the following uses:

Education, a Discretionary Use in accordance with Section 3.4.2 of Land Use Bylaw 944-19. This use includes:

- 750 square metres of floor area located on the second floor of the east building (E201) as shown on Drawing number DP2.1; and
- 420 square metres of outdoor play area, comprised of:
 - 315 square metres located on the roof above the first floor as shown on Drawing number DP2.1; and
 - 105 square metres located at grade between the east façade and the east property line as shown on Drawing number 2.0.

Restaurant/Café, a Discretionary Use in accordance with Section 3.4.2 of Land Use Bylaw 944-19. This use is authorized within units W101, W109, W110, E104, E105, E108, and E109 as shown on Drawing numbers DP2.0, and DP2.3.

Retail & Service – General, a Permitted Use in accordance with Section 3.4.2 of Land Use Bylaw 944-19. This use is authorized within the remaining floor area as shown on Drawing Numbers DP2.0, DP2.3, and DP2.4.

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Permit Number: 2025-077

The development and uses identified above are approved by the Development Authority, subject to the conditions of approval listed below. Unless otherwise expressly provided for in this Development Permit, all requirements of the City of Beaumont Land Use Bylaw 944-19 shall be met.

Development Authority Interpretation

It is the interpretation of the Development Authority that no provisions of Land Use Bylaw 944-19 have been relaxed, varied, or waived based on the following:

Section 3.5.8 of Land Use Bylaw 944-19 establishes a series of frontage types intended to shape the relationship between buildings and the public realm within the Integrated Neighbourhood District. The Development Authority interprets that the frontage types contemplated in this section are primarily oriented toward smaller-scale, lot-specific development forms and do not explicitly contemplate larger-scale multi-unit commercial development on a single site. As a result, no single frontage type is defined that directly corresponds to the scale, configuration, and site layout of the proposed development, despite that this form of development and corresponding uses are allowed within the district.

Notwithstanding, the Development Authority finds that the development demonstrates a clear effort to respond to the intent of the frontage type standards by aligning the development, where practicable, with the principles associated with a Shopfront frontage as described in Section 3.5.8.f.

The Development Authority notes that the development relates to the public realm by orienting multiple ground-floor commercial units toward the adjacent public streets. Four units have been designed to address Rue Bouchard, and one unit in each building has been oriented toward Coloniale Way. The north façades of both buildings incorporate veneer glazing intended to visually extend the appearance of a continuous shopfront condition and to enhance architectural articulation along Coloniale Way.

Taken together, the Development Authority is satisfied that the proposed development achieves the spirit and intent of Section 3.5.8, including the objective of creating street-oriented, visually engaging building frontages that enhance the public realm. Accordingly, the requirement in Section 3.5.8.f.vii that a minimum of 50 percent of the ground-floor façade be composed of windows is met in substance, and no variance, relaxation, or waiver is required.

Development Permit Conditions

Be sure to review all the documentation included with this permit.

1. Development shall commence within one year from the date of decision noted above, or within the period established within a time-extension agreement. If the development does not commence within this time frame, a new development permit will be required.
2. **As this permit has been issued for two Discretionary Uses, this permit shall not come into force and effect until the appeal period has expired, January 27, 2026.**
3. Changes or Alterations to the Development Permit

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Permit Number: 2025-077

- a. The site shall be developed in accordance with the attached plans issued for development dated January 5, 2026. **Any revisions to the approved plans require prior written approval by the City.**
 - b. This Development Permit authorizes the uses and unit locations expressly identified in the *Development Permit Authorization* section of this approval. Any change in use, expansion of use, or relocation of an approved use within either building, including a change to a different use category under Land Use Bylaw 944-19, shall require a separate Development Permit approval from the City of Beaumont prior to the commencement of the proposed change.
4. Prior to commencing any activity on the land, the owner/developer shall enter into and during the currency of the permit abide by a Development Agreement (pursuant to the Municipal Government Act s. 650), containing terms acceptable to the Municipality. The Development Agreement shall include but not be limited to the following:
- a. that the owner/developer submit civil engineering drawings in accordance with the City of Beaumont General Design Standards and to the satisfaction of the Municipality.
 - b. that the owner/developer shall provide security in a form satisfactory to the City for all obligations under the Development Agreement, including but not limited to, pre-grading, civil works on public property, and hard and soft landscaping on private property.
 - c. That any existing pathway located partially within the southwest portion of the parcel be removed and realigned so that it is located entirely within the public right-of-way. The removal, realignment, and design of the pathway shall be addressed through the Development Agreement and completed to the satisfaction of the City of Beaumont.
 - d. That the owner/developer design and construct a right-turn lane on the south side of Coloniale Way, extending from 50th Street to the primary site access on the north side of the development, including all required signage and pavement markings, in accordance with City of Beaumont standards and to the satisfaction of the City. The design and construction of the right-turn lane shall be addressed through the Development Agreement.
5. Any portion of the existing public trail located within the subject parcel shall be removed from the private land. The removal and relocation of the trail shall be carried out in accordance with an executed Development Agreement as noted in Condition 4(c) of this Development Permit, to the satisfaction of the City of Beaumont.
6. Landscaping:
- a. Landscaping shall be provided in accordance with the attached approved plans, except where those plans conflict with the City of Beaumont General Design Standards or approved tree species requirements, in which case the General Design Standards and approved species list shall prevail.

Development Permit Notice of Decision

Date of Decision: January 5, 2026

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Hard landscaping shall be contiguous and seamlessly integrated with the public sidewalk, with no grade adjustments.

- b. All trees proposed to be planted between the City's road right-of-way and the building façade, including those adjacent to municipal boulevards and municipal frontages and flankages, shall be selected from the City of Beaumont Approved Tree Species List – 2025 Edition. The owner/applicant is responsible for ensuring that all proposed trees are appropriate for their specific location.
- c. Any existing trees to be retained on the site or within the adjacent municipal boulevard shall be protected throughout all phases of construction in accordance with the City of Beaumont General Design Standards. Where sod or final landscaping will not be placed until completion of all construction activities, protection of boulevard trees is required.

The following minimum tree protection measures shall be implemented and maintained for the duration of construction:

- i. Trees located within 3 metres of active construction or construction traffic shall be protected on all sides with minimum 1.2 metre-high, 10 mm thick plywood fencing, installed a minimum of 1.0 metre from the trunk of the tree.
- ii. Trees located between 3 metres and 5 metres of active construction or construction traffic shall be protected on all sides with minimum 1.2 metre-high snow fencing, installed a minimum of 1.0 metre from the trunk of the tree.

Tree protection measures shall be installed prior to the commencement of construction activities and shall remain in place until construction is complete, to the satisfaction of the City of Beaumont.

- d. The owner/applicant shall update all applicable drawings to demonstrate compliance with this condition and submit the revised drawings to the City for review and acceptance prior to the commencement of any construction.
- 7. Wall-mounted lighting shall be installed along the building facades as indicated on the approved lighting plan. All lighting shall be directed downward and shielded to prevent spillover onto adjacent lots. All permanently installed lighting shall comply with International Dark-Sky Association requirements.
 - 8. All mechanical equipment shall be screened from view or integrated into the building roofline, where appropriate.
 - 9. The owner/developer shall ensure all snow is cleared and removed from the site, as no area has been approved for seasonal snow storage.
 - 10. The owner/developer shall provide the Development Authority with AutoCAD drawings, to its satisfaction, with the building permit application.
 - 11. The owner/developer shall obtain all federal, provincial and local permits as they apply to this project.

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Permit Number: 2025-077

12. This Development Permit does not authorize the removal, relocation, alteration, or modification of the Coloniale Estates Neighbourhood Entrance Sign. Any removal, relocation, or modification of the sign shall require separate written authorization from the City of Beaumont.

Additional Information

1. Separate sign permit applications will be required for any on-site signage.
2. Based on the details of the approved plans, the development meets the minimum required 75% Essential Elements and 25% Suggested Elements of the Beaumont Urban Design Guidelines.
3. Building Advisements
 - a. The owner/developer is responsible for submitting a building permit application. If the building drawings do not align with this Development Permit or require amendments due to building code compliance, a Development Permit amendment application (including fees) must be submitted. Building permit processing will pause until the amendment is approved.
 - b. The owner/developer shall provide spatial separation calculations for glazing or indicate whether the buildings are sprinklered, as this affects spatial separation requirements.
 - c. The owner/developer is advised that the City of Beaumont Fire Department's response time exceeds 10-minutes; the permit shall reflect this.
4. Engineering Advisements
 - a. The owner/developer shall be responsible for any engineering and legal costs incurred by the City related to this project.
 - b. The owner/developer shall provide additional blow-off and flush points for the onsite watermain, in accordance with the City of Beaumont General Design Standards and to the satisfaction of the Municipality.
5. Fire Advisements
 - a. A Fire Safety Plan must be posted in a visible area on the construction site.
 - b. The owner/developer shall purchase a key box from the City Hall Office to ensure all building units are accessible in case of an emergency.
6. Infrastructure Advisements
 - a. The applicant/owner shall place no parking signs along Coloniale Way and Rue Bouchard to the satisfaction of the Development Authority in consultation with the Infrastructure Department.
 - b. The owner/developer shall contact all franchise utilities to arrange for any service connections that are required. Where City utilities and services are interfered with or for construction, which is on

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Permit Number: 2025-077

- municipal property, the owner/developer will be responsible for the cost of relocation/repair of these municipal services.
- c. The water meter(s) for this project shall be purchased from the City of Beaumont. For each meter to be installed a "Water Meter Permit Request" must be completed electronically and submitted to waterandwastewater@beaumont.ab.ca. This application must be submitted thirty (30) days prior to occupancy. Size, type, and number of meters per building must be approved by the City of Beaumont.
 - d. The owner/developer shall include a right turn lane into the development with the required signage and line marking. The lane is located from 50th Street travelling eastbound to the development's entrance on Coloniale Way.
 - e. All tree species located adjacent to the City Boulevard shall be on the 2025 approved list for boulevards.
 - f. The owner/developer shall ensure that grading meets the City of Beaumont General Design Standards.
 - g. Applicant to confirm if there is an on site storm water management storage requirements for this parcel.
 - h. The existing Sanitary line may require up sizing, applicant is required to confirm the design flows and revisit the existing services. All tie-in connections affected by the upsizing work will need to be upgraded to meet City's current standards. Please refer to City's current GDS for more details on design guidelines.
 - i. Watermain into the site from Rue Bouchard will need to be upsized. The watermain coming into the property was a150mm. The General Design Standards section 3.4 Water Distribution and 3.4.1 Design Criteria states that the minimum diameter of watermain shall be 250 mm for commercial and light industrial developments. All tie-in connections affected by the upsizing work will need to be upgraded to meet City's current standards. Please refer to the City's current General Design Standards for more details on design guidelines.
 - j. Storm tie-in, and other utility improvements on public property, will need to be completed by a Beaumont approved contractor.
7. This Development Permit is issued under the City of Beaumont Land Use Bylaw 944-19. It does not exempt you from compliance with any other municipal bylaw or statutory plan applicable to the Proposed Development, any relevant federal or provincial statute or regulation, or any easement, covenant, agreement, or contract affecting the subject lands.
 8. This Notice of Decision is NOT a building permit. Work or construction shall not commence until an applicable Building Permit has been issued under the Alberta Safety Codes Act and any other applicable bylaws or regulations.

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Permit Number: 2025-077

9. Contact Alberta One Call at 1-800-242-3447 to locate underground services prior to construction, if applicable.
10. The site shall be kept clear of all construction garbage and debris; an on-site garbage container/bin shall be required.
11. Failure to keep the sites clean of debris is an offence under Our Zoning Blueprint. The Peace Officers may issue offence tickets to any person who has committed or is committing an offence respecting this infraction and may be subject to the following penalties:
 - a. First Offence – a written warning or a stop work order shall be issued, and a bin will be required onsite;
 - b. Second Offence (on same lot) – a minimum fine of \$1,000.00 and a stop work order shall be issued;
 - c. Third (and Subsequent) offence(s) (on same lot) – a minimum fine of \$5,000.00 and a stop work order shall be issued.
12. It is the responsibility of the owner/developer to ensure they have reviewed and understand all Instruments registered against the Title of the subject property. This includes all easements, caveats, and restrictive covenants. The City shall not address, nor enforce, any Instruments of which we have no interest in and/or are not a party to.

Permit Notification Information

It is the owner/developer's responsibility to manufacture and install 4'x6' signs on site notifying of the development, in accordance with the template provided by the City of Beaumont.

Furthermore, given that this Development Permit is for a development that may be of public interest, general information regarding this approved Permit may be published on the City of Beaumont website.

Appeal Information

Any Development Permit may be appealed to the Subdivision and Development Appeal Board (SDAB) if the permit was:

- a) issued for a permitted use with a variance, or for a discretionary use, or
- b) issued with conditions, or
- c) refused.

An appeal may be filed by:

- a) the person applying for the permit, and/or

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Permit Number: 2025-077

b) any person affected by the issuance of the permit.

As the person applying for the permit, you may appeal the decision of the Development Authority regarding the permit or any conditions placed on the permit (as listed above) within 21 days after the date on which the decision is made.

| | |
|---|------------------|
| Notice of Decision: | January 5, 2026 |
| Appeal deadline: | January 27, 2026 |
| Permit active (if no appeals filed): | January 28, 2026 |

Please be advised that an appeal may be submitted in accordance with Section 685 of the Municipal Government Act with the Subdivision and Development Appeal Board within 21 days of the written decision. To file an appeal or to get information on the appeal process you must contact the Secretary of the SDAB directly at 780-929-8782 or at legislative@beaumont.ab.ca. Appeals must be filed no later than 4:30 p.m. on the date indicated above. Please visit our website for more details at www.beaumont.ab.ca

For more information regarding this Development Permit, its conditions, or the Land Use Bylaw, contact the Development Authority who made the decision on this permit:



Craig Thomas, RPP, MCIP
Development Authority

cc:
Kendra Raymond, Director, Planning & Development
Jennifer Niesink, Director, Economic Development
Jay Bohachyk, Director, Finance
Jay Melvin, Director, Protective Services & Fire Chief
Aaron Lewicki, Director, Infrastructure
Paul Suiter, Director, Community Services
Punam Grewal, Manager, Engineering & Environment
Bryce Piacentini, Manager, Parks and Roads Operations
Ryan Orlovsky, Manager, Facility & Utility Operations
Joannes Wong, Manager, Long Range Planning
Yasmin Sharp, Manager, Development Services
Wendy Jones, Manager, Investment Attraction & Growth
Carley Krahn, Fire Prevention Officer
Joe Ross, Safety Codes Team Lead
Katrina Tarnawsky, Senior Planner, Land Development



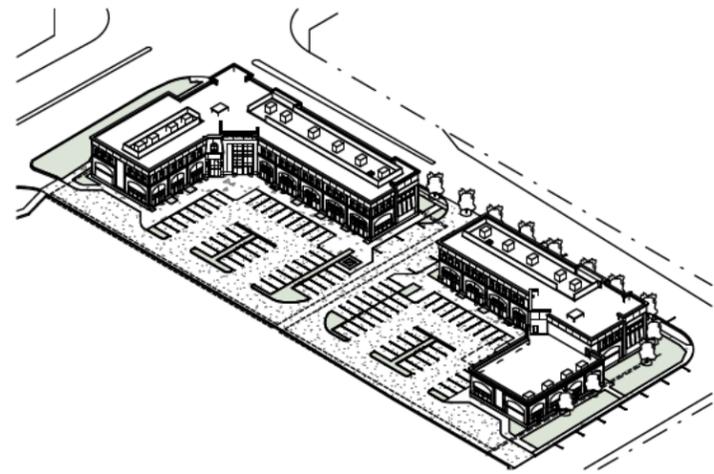
City of Beaumont
5600 - 49 Street
Beaumont, Alberta T4X 1A1
Phone: (780) 929-8782
Fax: (780) 929-3300
Email: development@beaumont.ab.ca

Development Permit Notice of Decision

Date of Decision: January 5, 2026

Permit Number: 2025-077

Craig Thomas, Senior Planner, Development Permitting
Krista Howe, Building Safety Codes Officer
Parth Mehta, Engineering Lead
Iolanda Troiani, Engineering Coordinator
Sara Edge, Operations Facility Administrative Assistant
Troy Birtles, Accurate Assessment
Dermian Ayalefac – Alberta Health Services - Leduc Public Health



MILLBREE
ARCHITECT ART AND DESIGN
 MILLBREE, CA 780.807.2294

ARCHITECTURAL / PRIME CONSULTANT:
 Millbree Architect Art and Design
CONTACT: Sabrina Reichinger
ADDRESS: 11116 - 127 Street NW
 Edmonton, AB T5M 0T4
PHONE: 780-807-2294

CIVIL CONSULTANT:
 DESIGN WORKS ENGINEERING LTD.
CONTACT: JATINDER JASSAL
ADDRESS: 10130 - 103 ST. NW, SUITE 1250
 EDMONTON, AB T5J 1Y7
PHONE: 780-278-7894

LANDSCAPE CONSULTANT:
 GREEN SPACE ALLIANCE
 DIVYANESH DESHPANDE / MARCELO FIGUEIRA
CONTACT: 10132 - 105 ST. NW
 EDMONTON, AB T5J 1C9
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 TAC STRUCTURAL INC.
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CONTACT: aaditya@tacstructural.com
ADDRESS: www.tacstructural.com
PHONE: 567.561.1126

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ARCHITECTURAL DRAWINGS

- DP0.0 TITLE SHEET
- DP1.0 SITE PLAN
- DP1.1 SITE DETAILS
- DP2.0 1ST FLOOR PLAN - CBE
- DP2.1 2ND FLOOR PLAN - CBE
- DP2.2 ROOF PLAN - CBE
- DP2.3 1ST FLOOR PLAN - CBW
- DP2.4 2ND FLOOR PLAN - CBW
- DP2.5 ROOF PLAN - CBW
- DP3.0 BUILDING ELEVATIONS - CBE
- DP3.1 BUILDING ELEVATIONS - CBE
- DP3.2 BUILDING ELEVATIONS - CBW
- DP3.3 BUILDING ELEVATIONS - CBW
- DP4.0 BUILDING SECTIONS - CBE
- DP4.1 BUILDING SECTIONS - CBW

CIVIL DRAWINGS

- C100 COVER SHEET
- C200 SITE GRADING PLAN - 1
- C201 SITE GRADING PLAN - 2
- C300 SITE SERVICING PLAN

LANDSCAPE DRAWINGS

- L-101 SITE PLAN
- L-102 PLANTING PLAN
- L-103 DETAILS

ELECTRICAL DRAWINGS

- E1.1 SITE PLAN

BELLEVIEW CENTRE

9 COLONIALE WAY, BEAUMONT, AB

ISSUE FOR DP

2025.06.23

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Issue / Permit Stamp

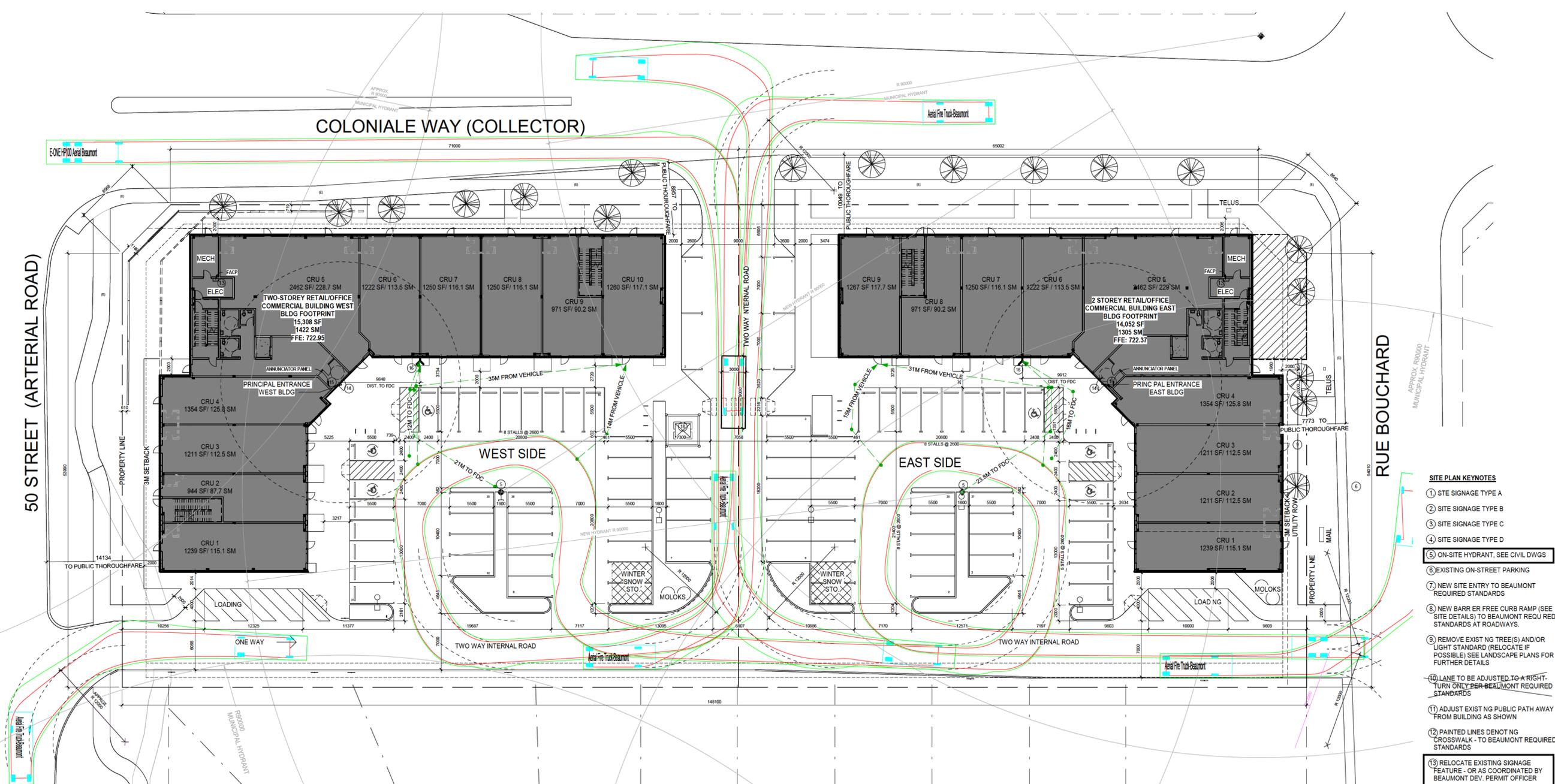
COLONIALE WAY
 COMMERCIAL
 9 COLONIALE WAY, BEAUMONT, AB

TITLE SHEET

Date: 2025-06-23
 Drawn by: — Checked by: SK
 Scale: File: 25-004
 Sheet No: **DP0.0**

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- SITE PLAN KEYNOTES**
- 1 SITE SIGNAGE TYPE A
 - 2 SITE SIGNAGE TYPE B
 - 3 SITE SIGNAGE TYPE C
 - 4 SITE SIGNAGE TYPE D
 - 5 ON-SITE HYDRANT, SEE CIVIL DWGS
 - 6 EXISTING ON-STREET PARKING
 - 7 NEW SITE ENTRY TO BEAUMONT REQUIRED STANDARDS
 - 8 NEW BARRIER FREE CURB RAMP (SEE SITE DETAILS) TO BEAUMONT REQUIRED STANDARDS AT ROADWAYS.
 - 9 REMOVE EXISTING TREE(S) AND/OR LIGHT STANDARD (RELOCATE IF POSSIBLE) SEE LANDSCAPE PLANS FOR FURTHER DETAILS
 - 10 LANE TO BE ADJUSTED TO A RIGHT-TURN ONLY PER BEAUMONT REQUIRED STANDARDS
 - 11 ADJUST EXISTING PUBLIC PATH AWAY FROM BUILDING AS SHOWN
 - 12 PAINTED LINES DENOTING CROSSWALK - TO BEAUMONT REQUIRED STANDARDS
 - 13 RELOCATE EXISTING SIGNAGE FEATURE - OR AS COORDINATED BY BEAUMONT DEV. PERMIT OFFICER
 - 14 PRINCIPAL ENTRANCE
 - 15 ANNUNCIATOR PANEL
 - 16 FIRE DEPARTMENT CONNECTION (FDC)
 - 17 EXISTING BU LID NG TO BE DEMOLISHED
 - 18 PAD MOUNT TRANSFORMER - PROVIDE DROPPED CURB IN FRONT PER UTILITY PROVIDER REQUIREMENTS.
 - 19 SITE LIGHTS - SEE ELEC DWGS
 - 20 6 BIKE STALLS - 'HOOP' STYLE BIKE RACKS. BLACK
 - 21 GAS METER W/ PROTECTIVE BOLLARDS - REFER TO MECHANICAL DRAWINGS.
 - 22 PROVIDE (2) M-5000 MOLOK UNDERGROUND WASTE CONTAINERS (OR APPROVED ALTERNATE) - REFER TO SITE DETAILS FOR INSTALLATION. PROVIDE (1) STANDARD WASTE LID (BLACK) AND (1) STANDARD LID WITH CARDBOARD SLOT (BLACK) AND BLUE USER LID. FRAMING TO BE CEDAR. SEMI-HARD L NERS.
 - 23 M-GREASE MOLOK UNDERGROUND WASTE CONTAINER - SEE ALSO ELECTRICAL DRAWINGS.
 - 24 OUTDOOR SPACE FOR FUTURE TENANT - REFER TO CIVIL AND LANDSCAPE DRAWINGS.
 - 25 WINTER SNOW STORAGE LOCATION - PRIVATELY CONTRACTED
 - 26 SITE SIGNAGE TYPE E
 - 27 SITE SIGNAGE TYPE F

2 SITE PLAN - FIRE TRUCK ACCESS
DP1.2-R3 1:250



SNOW REMOVAL IS EXPECTED TO BE PRIVATELY CONTRACTED USING A SKID-STEER VEHICLE. TURNING RADIUS IS APPROX 1.5M, AND NOT EXPECTED TO BE AN ISSUE WITH LOCATION ON SITE PLAN.



ZONING INTEGRATED NEIGHBOURHOOD DISTRICT
LEGAL DESCRIPTION PLAN 912 1656, BLOCK 103, LOT 1
MUNICIPAL ADDRESS 9 COLONIALE WAY, BEAUMONT, ALBERTA
SITE AREA 0.8857 HA / 2.1885 ACRES (95,332 SF/8857 SM)

BUILDING
COMMERCIAL BUILDING WEST (CBW) BUILDING AREA = 1422 SM
 2 STOREYS ABOVE GRADE (NO BASEMENT)
 CLASSIFICATION: 3.2.2.71 GROUP E, UP TO 2 STOREYS, SPRINKLERED

COMMERCIAL BUILDING EAST (CBE) BUILDING AREA = 1305 SM
 2 STOREYS ABOVE GRADE (NO BASEMENT)
 CLASSIFICATION: 3.2.2.71 GROUP E, UP TO 2 STOREYS, SPRINKLERED

LOCATION OF FDC = KEYNOTE 16
 LOCATION OF FIRE ALARM CONTROL PANEL = KEYNOTE 13
 LOCATION OF ANNUNCIATOR PANEL = KEYNOTE 15
 LOCATION OF PRINCIPAL ENTRANCE = KEYNOTE 14

FIRE TRUCK BEAUMONT DIMENSIONS: 9M LONG X 3M W WDE



Consultant

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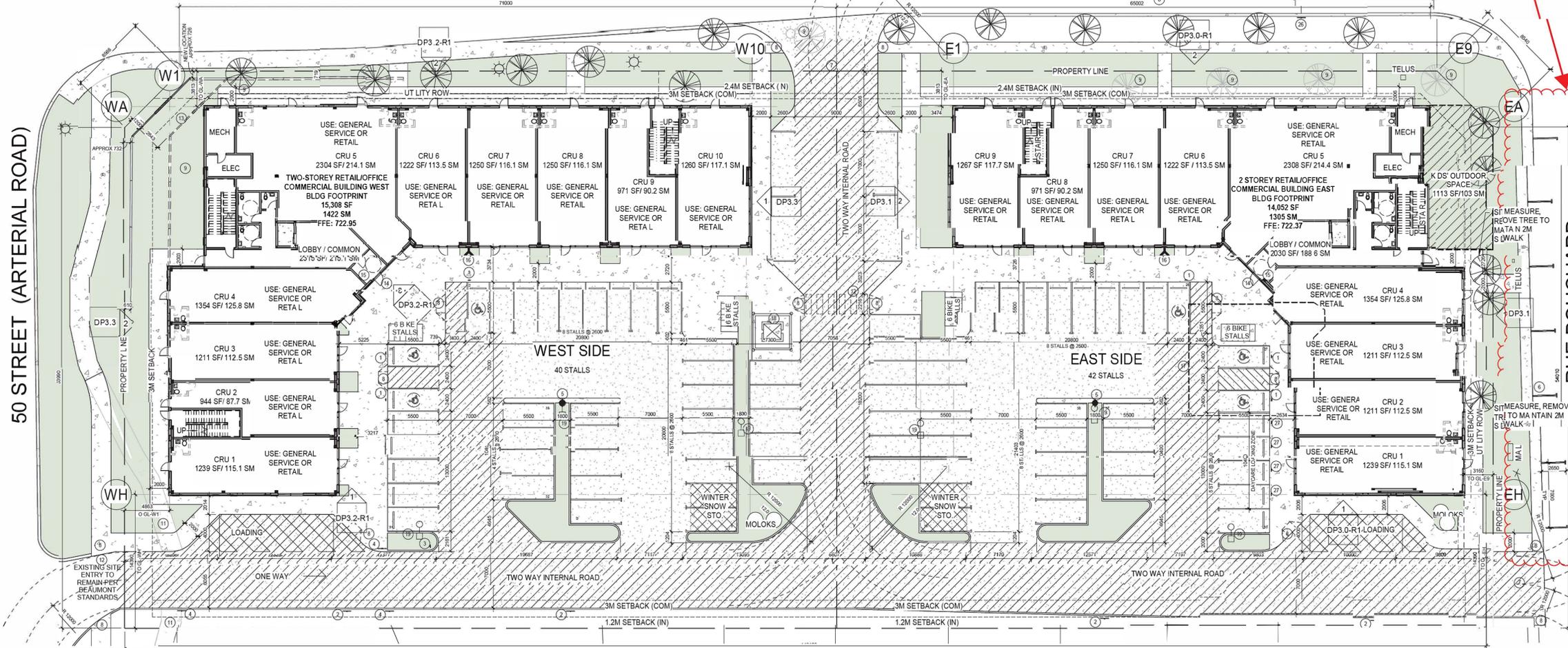
COLONIALE WAY COMMERCIAL
9 COLONIALE WAY, BEAUMONT, AB

FIRE ACCESS

| | |
|----------|--------------------------|
| Date | 2025-10-31 |
| Drawn by | Checked by SK |
| Scale | As indicated File 25-004 |
| Sheet No | |

DP1.2-R3

COLONIALE WAY (COLLECTOR)



SITE LEGEND

- LANDSCAPED AREA SEE LANDSCAPE PLANS
- EXIST NG CONCRETE
- NEW CONCRETE
- ASPHALT
- AREA FOR WINTER SNOW STORAGE
- DENOTES HEAVY DUTY ASPHALT

- SITE PLAN KEYNOTES**
- SITE SIGNAGE TYPE A
 - SITE SIGNAGE TYPE B
 - SITE SIGNAGE TYPE C
 - SITE SIGNAGE TYPE D
 - ON-SITE HYDRANT, SEE CIVIL DWGS
 - EXIST NG ON-STREET PARK NG
 - NEW SITE ENTRY TO BEAUMONT REQU RED STANDARDS
 - NEW BARRIER FREE CURB RAMP (SEE SHEET DETAILS) TO BEAUMONT REQUIRED STANDARDS AT ROADWAYS.
 - REMOVE EXISTING TREE(S) AND/OR LIGHT STANDARD (RELOCATE IF POSSIBLE) SEE LANDSCAPE PLANS FOR FURTHER DETAILS
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 - RELOCATE EXISTING SIGNAGE FUTURE - OR AS COORD NATED BY BEAUMONT DEV. PERMIT OFFICER
 - PRINCIPAL ENTRANCE
 - ANNUNCIATOR PANEL
 - FIRE DEPARTMENT CONNECTION (FDC)
 - EXISTING BUILDING TO BE DEMOLISHED
 - PAD MOUNT TRANSFORMER - PROVIDE DROPPED CURB IN FRONT PER UTILITY PROVIDER REQUIREMENTS.
 - SITE LIGHTS - SEE ELEC DWGS
 - 6 B E STALLS - 'HOOP' STYLE BIKE RACKS, BLACK
 - GAS METER W/ PROTECTIVE BOLLARDS - REFER TO MECHANICAL DRAWINGS.
 - PROVIDE (2) M-5000 MOLOK UNDERGROUND WASTE CONTAINERS (OR APPROVED ALTERNATE) - REFER TO SITE DETAILS FOR INSTALLATION. PROVIDE (1) STANDARD WASTE LD (BLACK) AND (1) STANDARD LD WITH CARDBOARD SLOT (BLACK) AND BLUE USER LD. FRAMING TO BE 'CEDAR', SEMI-HARD LINERS.
 - M-GREASE MOLOK UNDERGROUND WASTE CONTAINER - SEE ALSO ELECTRICAL DRAWINGS.
 - OUTDOOR SPACE FOR FUTURE TENANT - REFER TO CIVIL AND LANDSCAPE DRAWNGS.
 - WINTER SNOW STORAGE LOCATION - PRIVATELY CONTRACTED
 - SITE SIGNAGE TYPE E
 - SITE SIGNAGE TYPE F

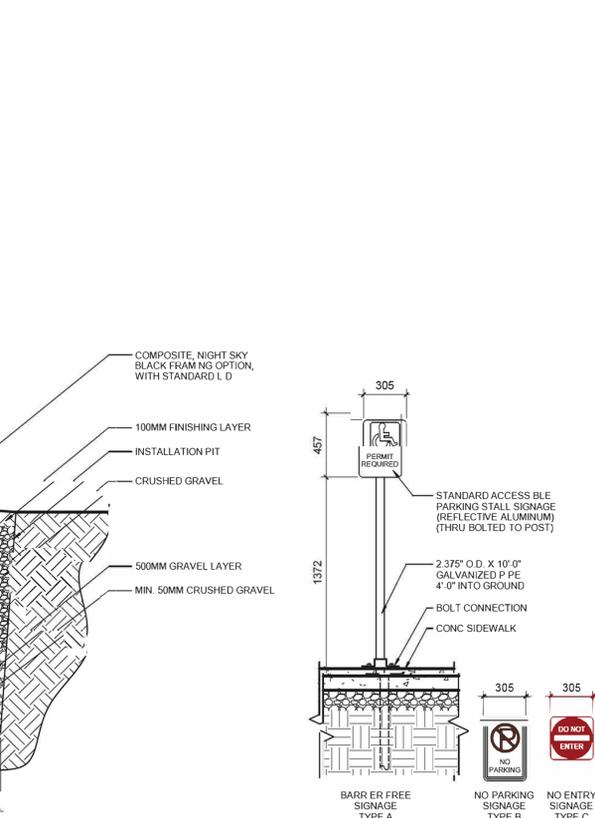
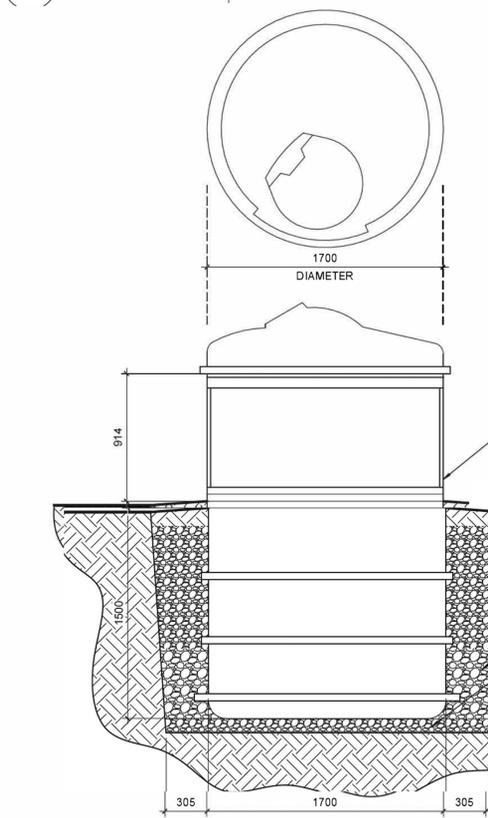


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1 SITE PLAN
DP1.0-R2 1:250



SITE STATS

ZONING: INTEGRATED NEIGHBOURHOOD DISTRICT
 LEGAL DESCRIPTION: PLAN 912 1656, BLOCK 103, LOT 1
 MUNICIPAL ADDRESS: 9 COLONIALE WAY, BEAUMONT, ALBERTA
 SITE AREA: 0.8857 HA / 2.1885 ACRES (95,332 SF/8857 SM)

| SITE STATS - WEST SIDE | | SITE STATS - EAST SIDE | |
|--|--------------------------------------|--|--------------------------------------|
| WEST SIDE AREA | 0.4609 HA / 1.1389 ACRES (49,610 SF) | EAST SIDE AREA | 0.4248 HA / 1.0496 ACRES (45,722 SF) |
| BLDG FLOOR AREA | | BLDG FLOOR AREA | |
| 1ST FLOOR: | | 1ST FLOOR: | |
| CRU | 12,991 SF / 1207 SM | CRU | 12,017 SF 1116.4 SM |
| LOBBY / CIRC. / UTIL. | 2315 SF / 215 SM | LOBBY / CIRC. / UTIL. | 2030 SF 188.6 SM |
| 2ND FLOOR: | | 2ND FLOOR: | |
| OFFICE SPACE | 13,960 SF / 1297 SM | DAYCARE | 8097 SF 752 SM |
| LOBBY / CIRC. / UTIL. | 1313 SF / 122 SM | LOBBY / CIRC. / UTIL. | 1015 SF 94 SM |
| TOTAL 2ND FLOOR: | 15,273 SF / 1419 SM | TOTAL 2ND FLOOR: | 9112 SF 847 SM |
| TOTAL FOOTPRINT | 15,308 SF / 1422 SM | TOTAL FOOTPRINT | 14,052 SF 1305 SM |
| TOTAL | 30,580 SF / 2841 SM | TOTAL | 23,164 SF 2152 SM |
| NET LEASABLE AREA | 26,951 SF / 2503 SM | NET LEASABLE AREA | 20,114 SF 1869 SM |
| SITE COVERAGE | 16% of site (30.9% of West side) | SITE COVERAGE | 14.7% of site (30.7% of East side) |
| FAR | 0.32 of site (0.616 of West side) | FAR | 0.24 of site (0.51 of East side) |
| PARKING PROVIDED | 40 STALLS | PARKING PROVIDED | 42 STALLS (54 W/ ON STREET PARK NG) |
| BYLAW PARK NG REQU RED | 25 STALLS: | BYLAW PARK NG REQU RED | 27 STALLS: |
| GEN. SERVICE AND RETAIL = | 1207 SM = 12 STALLS | GEN. SERVICE AND RETAIL = | 1116.4 SM = 12 STALLS |
| OFFICE (2ND FLOOR) = | 1297 SM = 13 STALLS | OFFICE/EDUCATION (2ND FLOOR) = | 752 SM = 15 STALLS |
| PARKING PER 1000 SF (COMMERCIAL/OFFICE) | 1.51 STALLS/1000 SF | PARKING PER 1000 SF (COMMERCIAL/OFFICE) | 2.10 STALLS/1000 SF |
| BIKE STALLS REQUIRED: 6 | | BIKE STALLS REQUIRED: 6 | |
| BIKE STALLS PROVIDED: 12 | | BIKE STALLS PROVIDED: 12 | |
| LOADING STALLS REQUIRED: 1 | | LOADING STALLS REQUIRED: 1 | |
| LOADING STALLS PROVIDED: 1 | | LOADING STALLS PROVIDED: 1 | |
| COMBINED TOTAL BUILD NG (FOOTPRINT) AREA = | 29,360 SF / 2727 SM | COMBINED TOTAL BUILD NG (FOOTPRINT) AREA = | 29,360 SF / 2727 SM |
| COMBINED TOTAL FLOOR AREA = | 53,744 SF / 4993 SM | COMBINED TOTAL FLOOR AREA = | 53,744 SF / 4993 SM |
| COMBINED SITES TOTAL NET LEASABLE = | 47,065 SF / 4372 SM | COMBINED SITES TOTAL NET LEASABLE = | 47,065 SF / 4372 SM |
| TOTAL PAVED AREA = | 3800 SM (43% OF SITE) | TOTAL PAVED AREA = | 3800 SM (43% OF SITE) |

Integrated Neighbourhood District

Density: For planned development: 35 units per net hectare or as per the applicable area structure plan, neighbourhood structure plan, or outline plan. Shadow plans of future phases of development shall be required to demonstrate how the minimum density is achieved.

Principal Frontage Setback: i. 3m min
ii. Principal frontage setbacks shall be different from the adjacent lot for single-detached dwellings.

Secondary Frontage Setback: i. Min 2.4 m when adjacent to a public roadway or 1.2 m when adjacent to a lane to max 4 m

Side Yard Setback: i. Min 1.2 m to max 4 m except for attached buildings where side yard setback is 0 m on the attached side

Zero Side Yard Standards: Min 1.5 m setback where other side yard is 0 m. A private maintenance easement shall be registered on titles adjacent to the zero lot line that provide a 0.30 m

Integrated Neighbourhood District

Parking: Residential Uses - 1 stall per unit over 75 sqm
Business Uses: Office - 1 stall per 100 sqm of lot coverage
Commercial Uses: Restaurant / Cafe - 2 stalls per 100 sqm of lot coverage
Retail and Service - General - 1 stall per 100 sqm of lot coverage
Institutional Uses: Education - 2 stalls per 100 sqm of lot coverage

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COLONIALE WAY COMMERCIAL
9 COLONIALE WAY, BEAUMONT, AB

SITE PLAN

Date: 2025-10-27
 Drawn by: SK
 Scale: As indicated
 Sheet No: 25-004

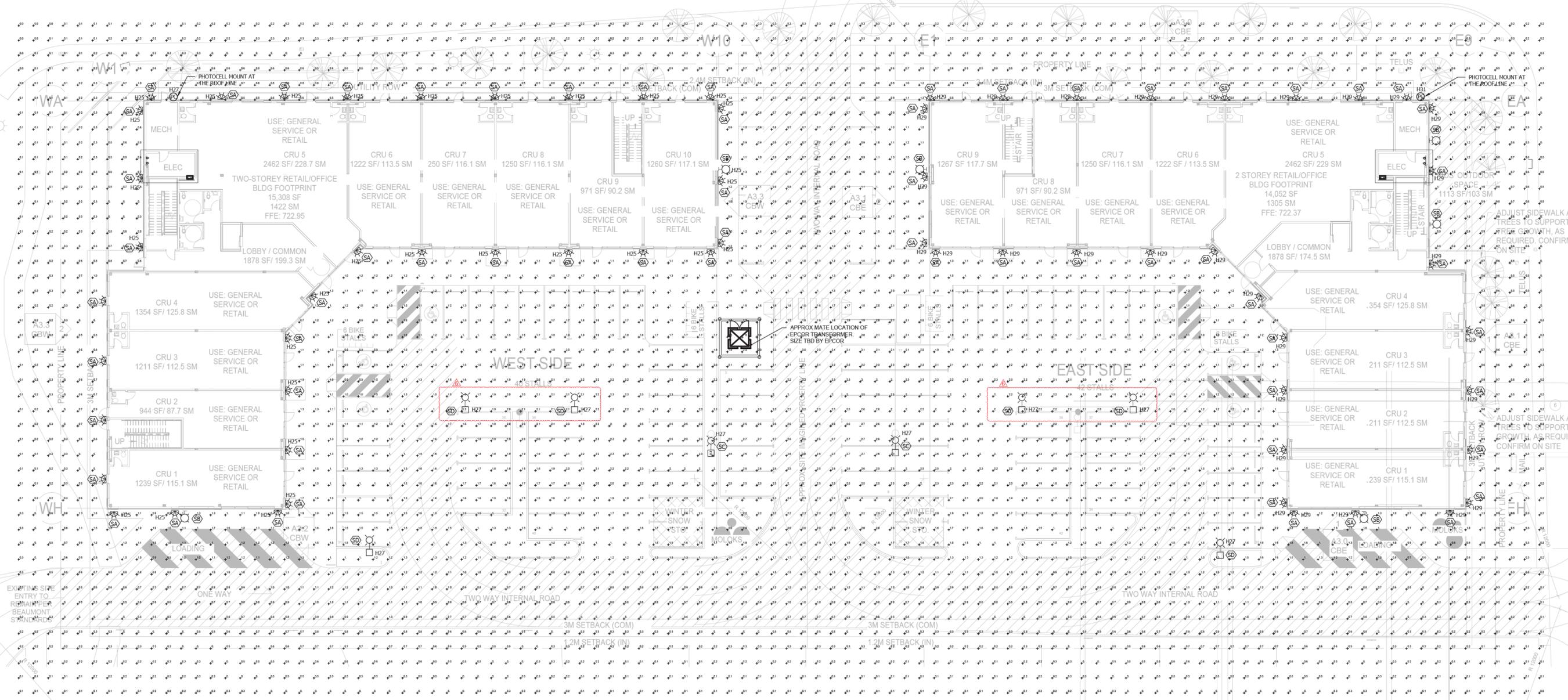
DP1.0-R2



2 MOLOK CLASSIC M500 DETAIL
DP1.0-R2 1:24

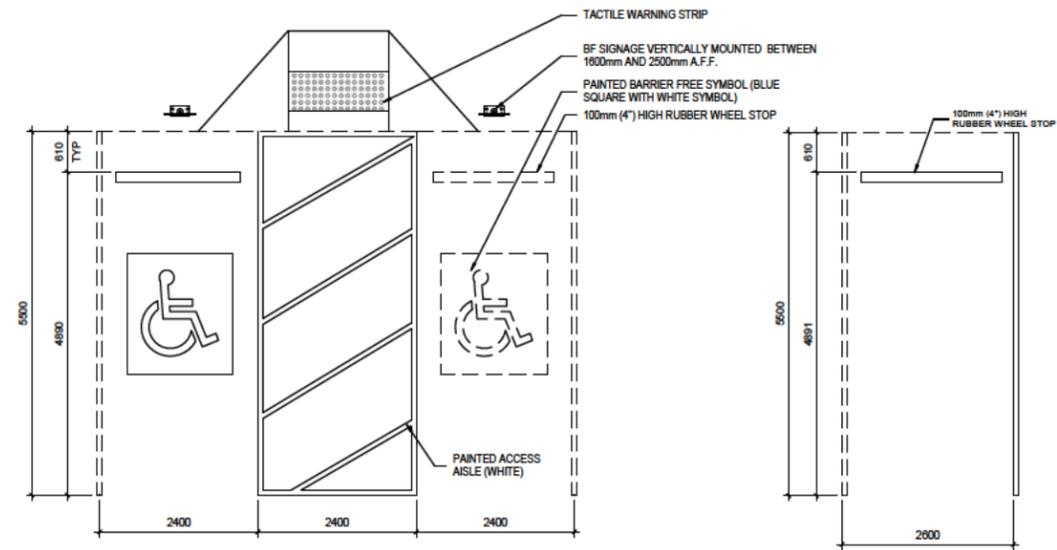
3 TYPICAL SIGN DETAILS
DP1.0-R2 1:24

COLONIALE WAY (COLLECTOR)



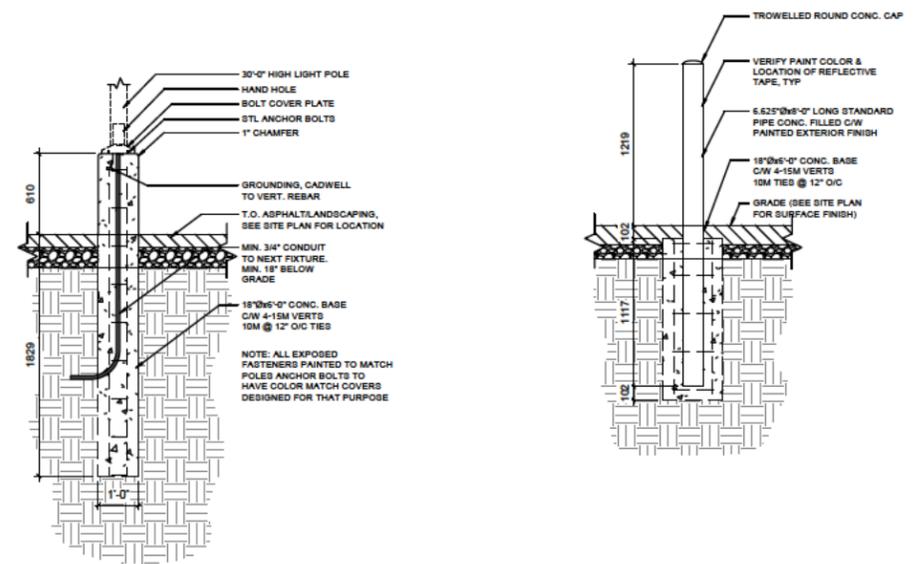
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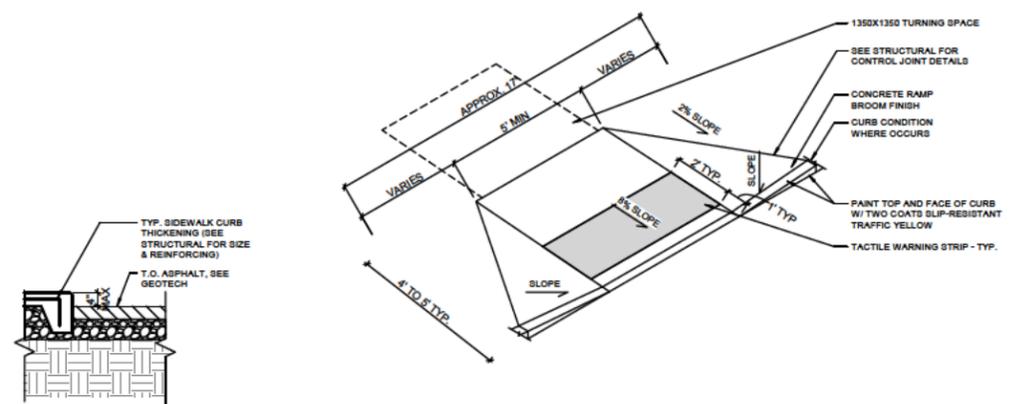
1 TYPICAL BARRIER FREE STALL
DP1.1 1:50

2 TYPICAL PARKING STALL
DP1.1 1:50



3 TYPICAL LIGHT SECTION
DP1.1 1:25

4 TYPICAL BOLLARD SECTION
DP1.1 1:25



5 TYPICAL CURB DETAIL
DP1.1 1:25

6 TYPICAL CURB CUT
DP1.1 1:100

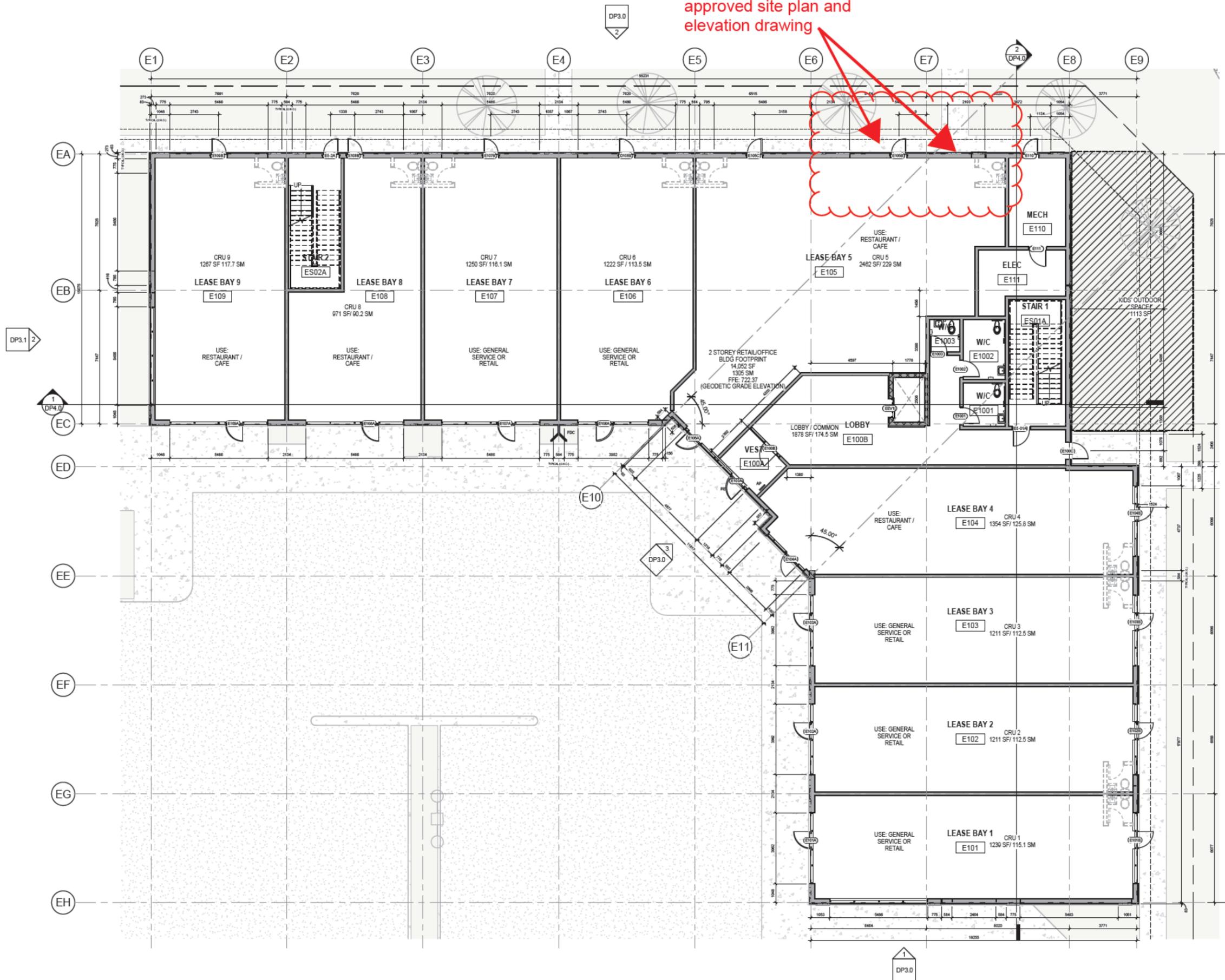
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COLONIALE WAY COMMERCIAL
9 COLONIALE WAY, BEAUMONT, AB

SITE DETAILS

| | |
|-------------|--------------|
| Date: | 2025-06-23 |
| Drawn by: | SK |
| Checked by: | SK |
| Scale: | As indicated |
| File: | 25-004 |
| Sheet No.: | DP1.1 |

Window opening as per approved site plan and elevation drawing



- FLOOR PLAN LEGEND**
- AP ANNUNCIATOR PANEL
 - FDC FIRE DEPARTMENT CONNECTION
 - PE PRINCIPAL ENTRANCE
- GENERAL NOTES**
1. SEE A0.1 FOR ALL NOTES, SYMBOL LEGENDS, AND ASSEMBLY TYPE
 2. FOR DOOR AND WINDOW INFORMATION SEE DOOR AND WINDOW SCHEDULES
 3. FIRE EXTINGUISHER LOCATIONS- SEE MECH



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9 COLONIALE WAY, BEAUMONT, AB

1ST FLOOR PLAN - CBE

| | |
|-----------|-----------------------|
| Date | 2025-06-23 |
| Drawn by | Checked by SK |
| Scale | As indicated / 25-004 |
| Sheet No. | DP2.0 |



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APPROVED
DEVELOPMENT PERMIT ONLY

[Redacted Signature]

Craig Thomas, RPP, MCIP
Development Authority

BEAUMONT
Date: **January 5, 2026**



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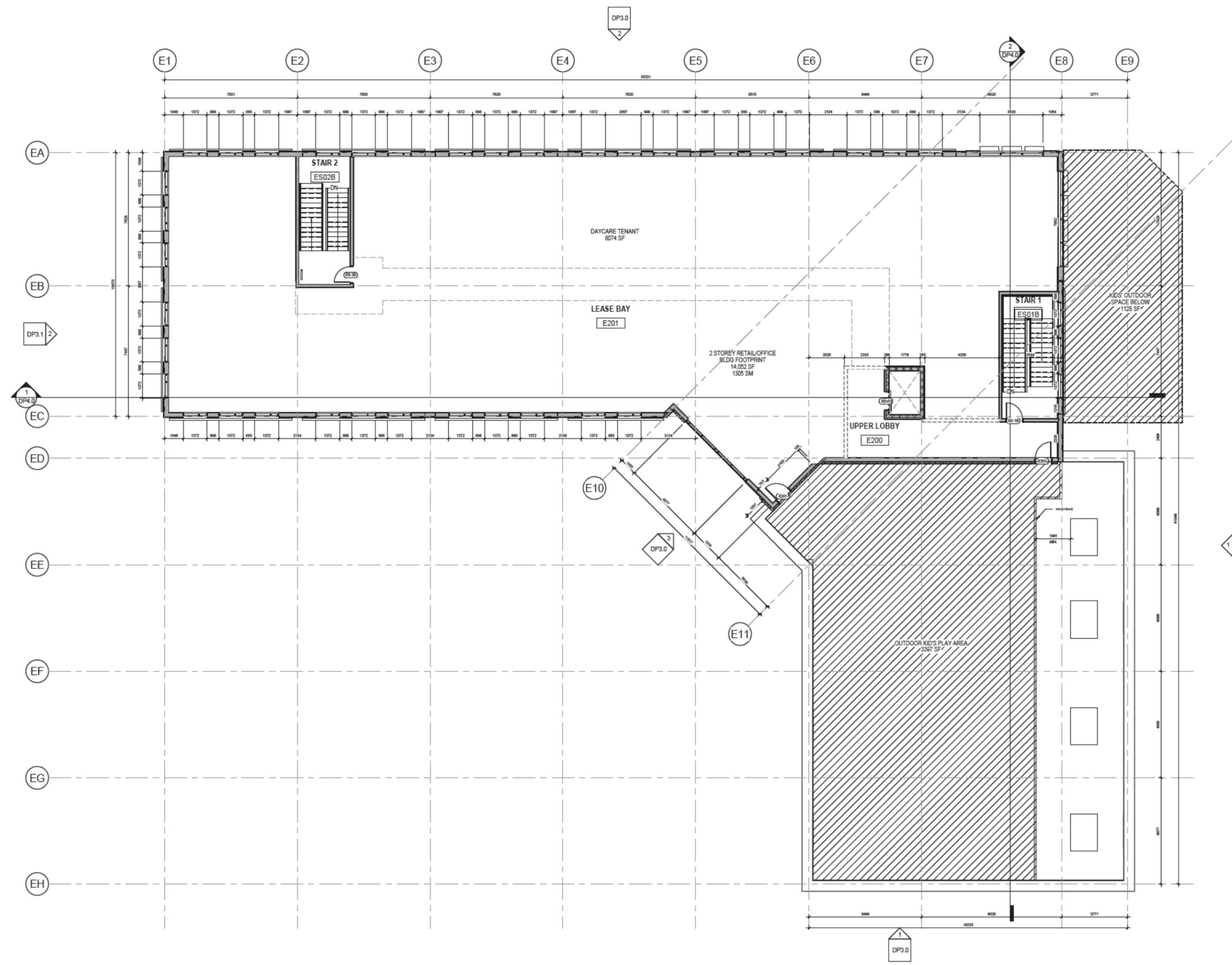
Issue / Permit Stamp

COLONIALE WAY COMMERCIAL
9 COLONIALE WAY, BEAUMONT, AB

2ND FLOOR PLAN - CBE

| | |
|------------|------------|
| Date: | 2025-06-23 |
| Drawn by: | SK |
| Scale: | 1:100 |
| Sheet No.: | 25-004 |

DP2.1



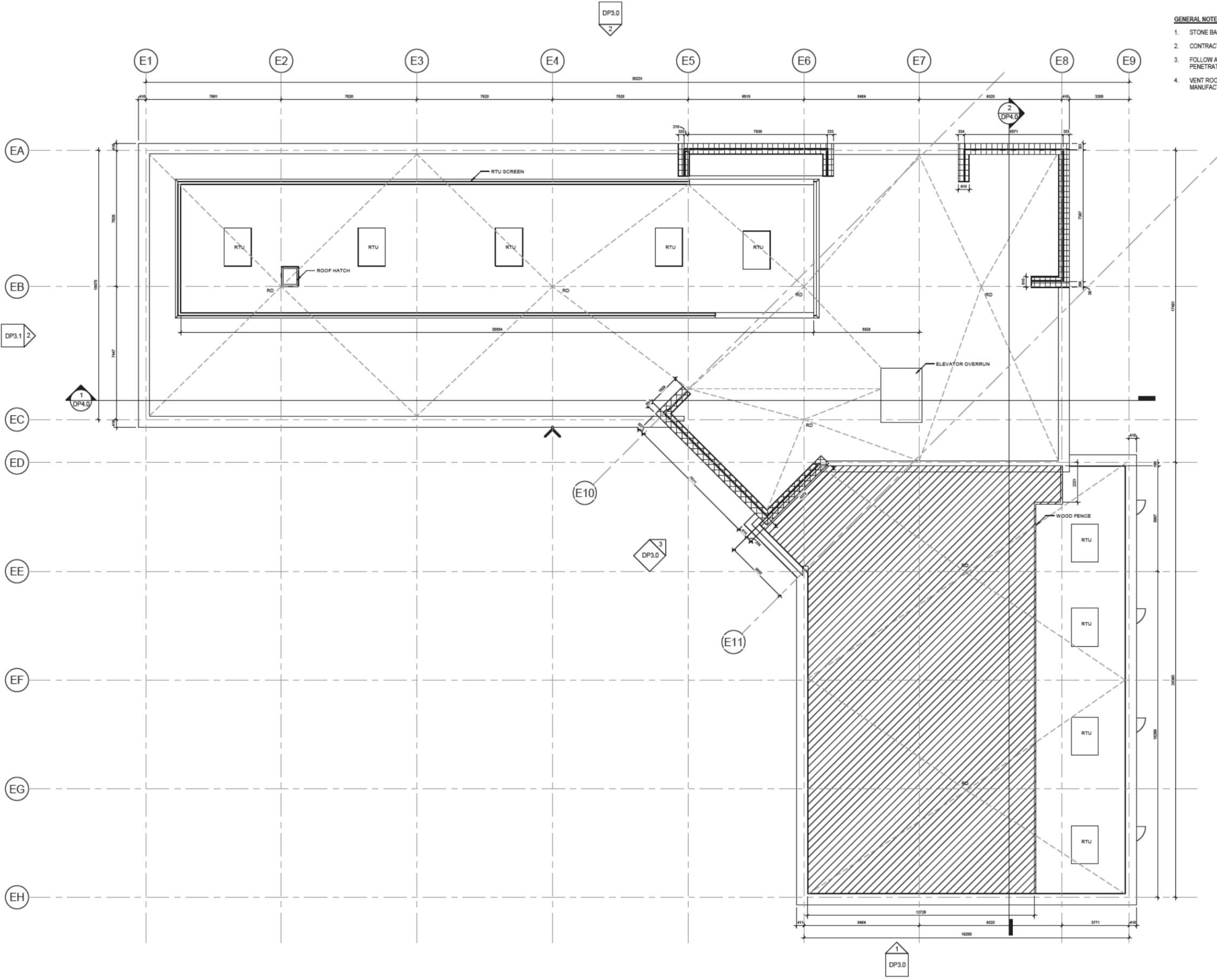
DP3.0
2

- GENERAL NOTES**
- STONE BALLAST EPDM ROOFING UNLESS NOTED OTHERWISE
 - CONTRACTOR TO SEE ELEC AND MECH. TYP.
 - FOLLOW ARCA STANDARDS DETAILS MANUAL FOR ALL ROOF PENETRATIONS AND ROOF DETAILS.
 - VENT ROOF/SOFFIT AREAS AS PER THE NBC (AE) 2023 AND MANUFACTURER RECOMMENDATIONS.



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COLONIALE WAY
COMMERCIAL
9 COLONIALE WAY, BEAUMONT, AB

ROOF PLAN - CBE

| | |
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| Date | 2025-06-23 |
| Drawn by | SK |
| Scale | As indicated |
| Sheet No. | 25-004 |

DP2.2

Window opening as per approved site plan and elevation drawings

- FLOOR PLAN LEGEND**
- AP ANNUNCIATOR PANEL
 - FDC FIRE DEPARTMENT CONNECTION
 - PE PRINCIPAL ENTRANCE
- GENERAL NOTES**
1. SEE A0.1 FOR ALL NOTES, SYMBOL LEGENDS, AND ASSEMBLY TYPE
 2. FOR DOOR AND WINDOW INFORMATION SEE DOOR AND WINDOW SCHEDULES
 3. FIRE EXTINGUISHER LOCATIONS- SEE MECH



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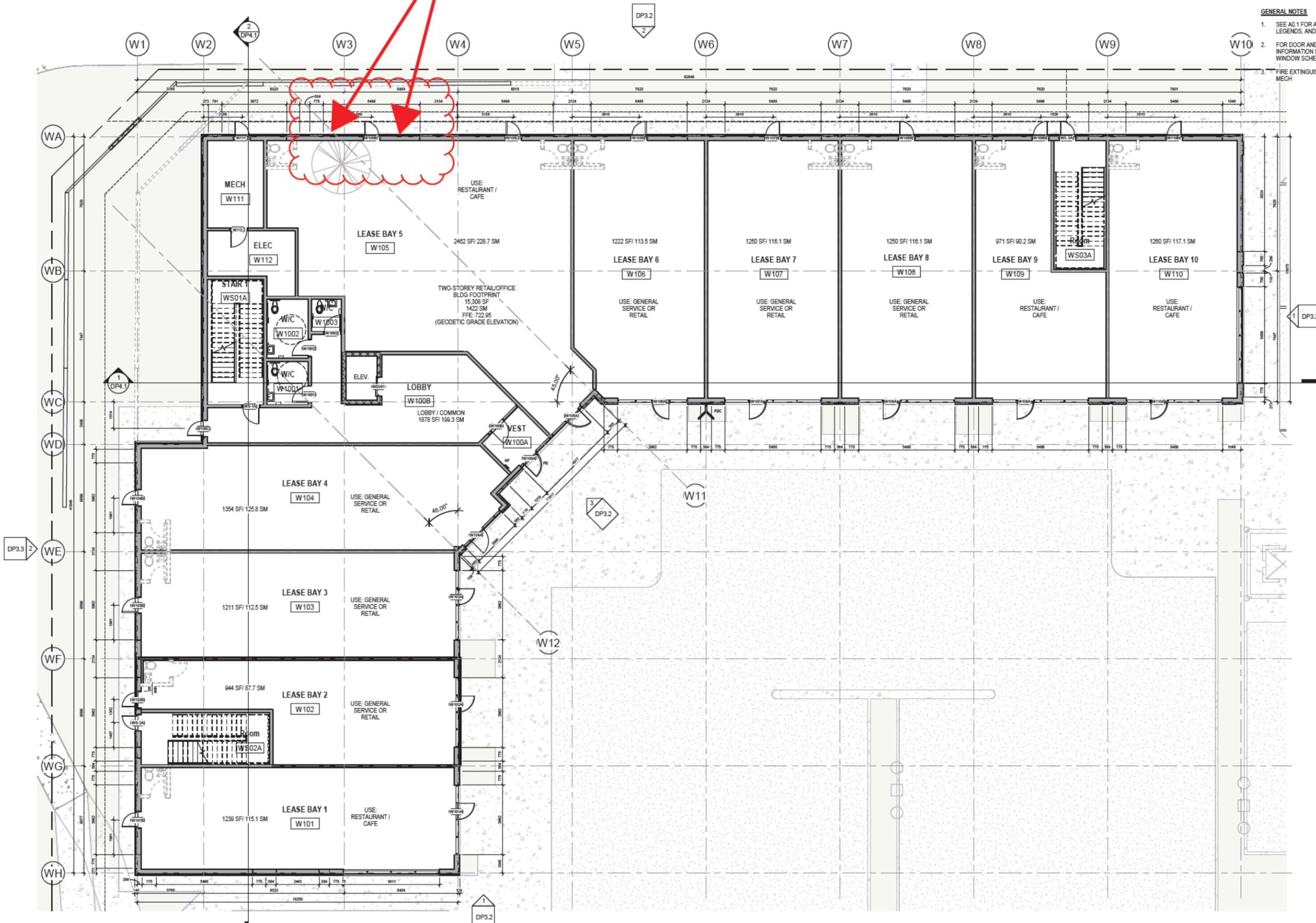
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Craig Thomas, Development Permit Specialist

Date: **January 5, 2026**



1 CBW - T/O LEVEL 1
DP2.3 1:100

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9 COLONIALE WAY, BEAUMONT, AB

1ST FLOOR PLAN -
CBW

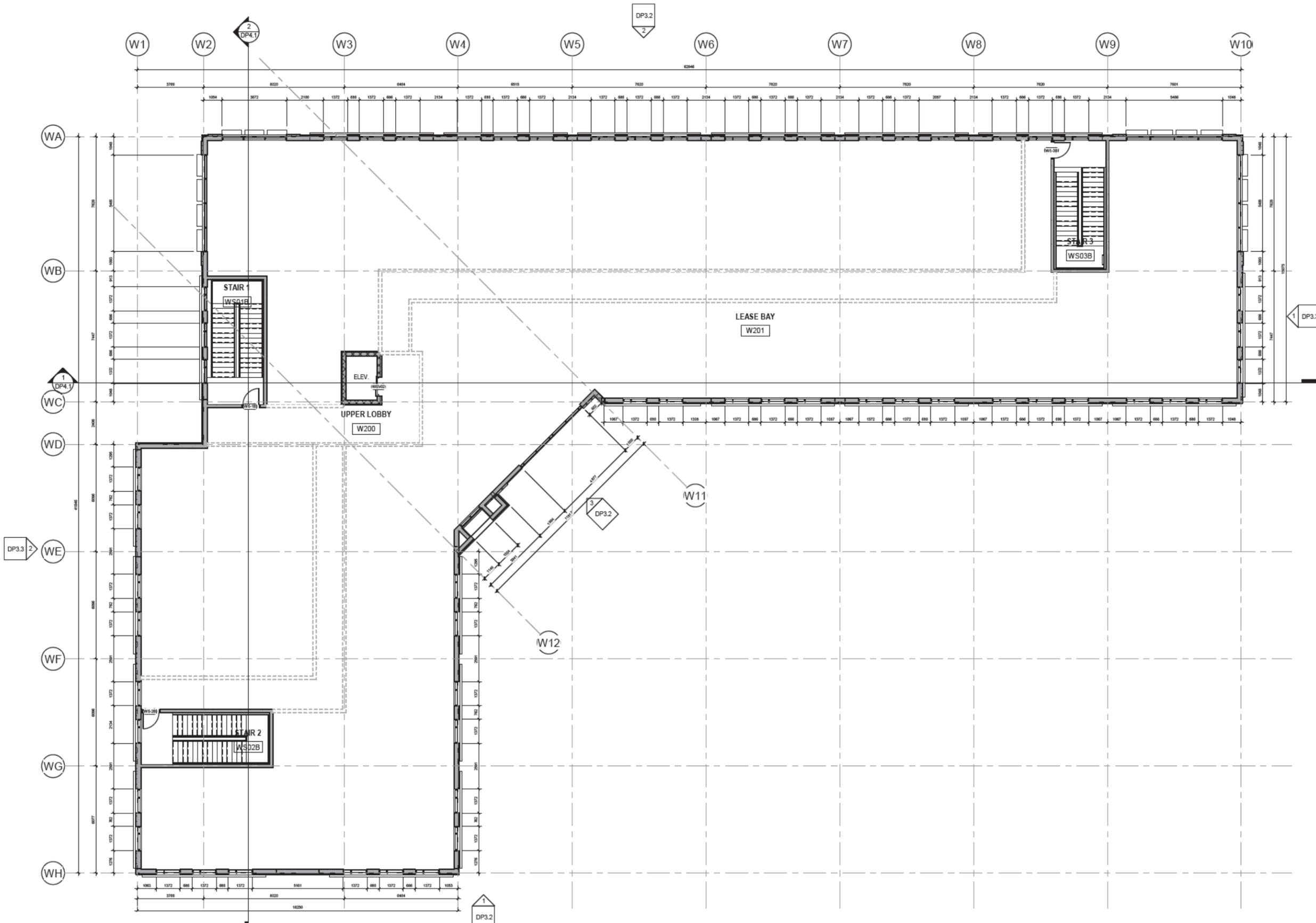
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1 CWB - T/O LEVEL 2
DP2.4 1:100

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2ND FLOOR PLAN -
CBW

Date: 2025-06-23
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Scale: 1:100 Plot: 25-004
Sheet No: **DP2.4**

- GENERAL NOTES**
1. STONE BALLAST EPDM ROOFING UNLESS NOTED OTHERWISE
 2. CONTRACTOR TO SEE ELEC AND MECH. TYP.
 3. FOLLOW ARCA STANDARDS DETAILS MANUAL FOR ALL ROOF PENETRATIONS AND ROOF DETAILS.
 4. VENT ROOF/SOFFIT AREAS AS PER THE NBC (AE) 2023 AND MANUFACTURER RECOMMENDATIONS.



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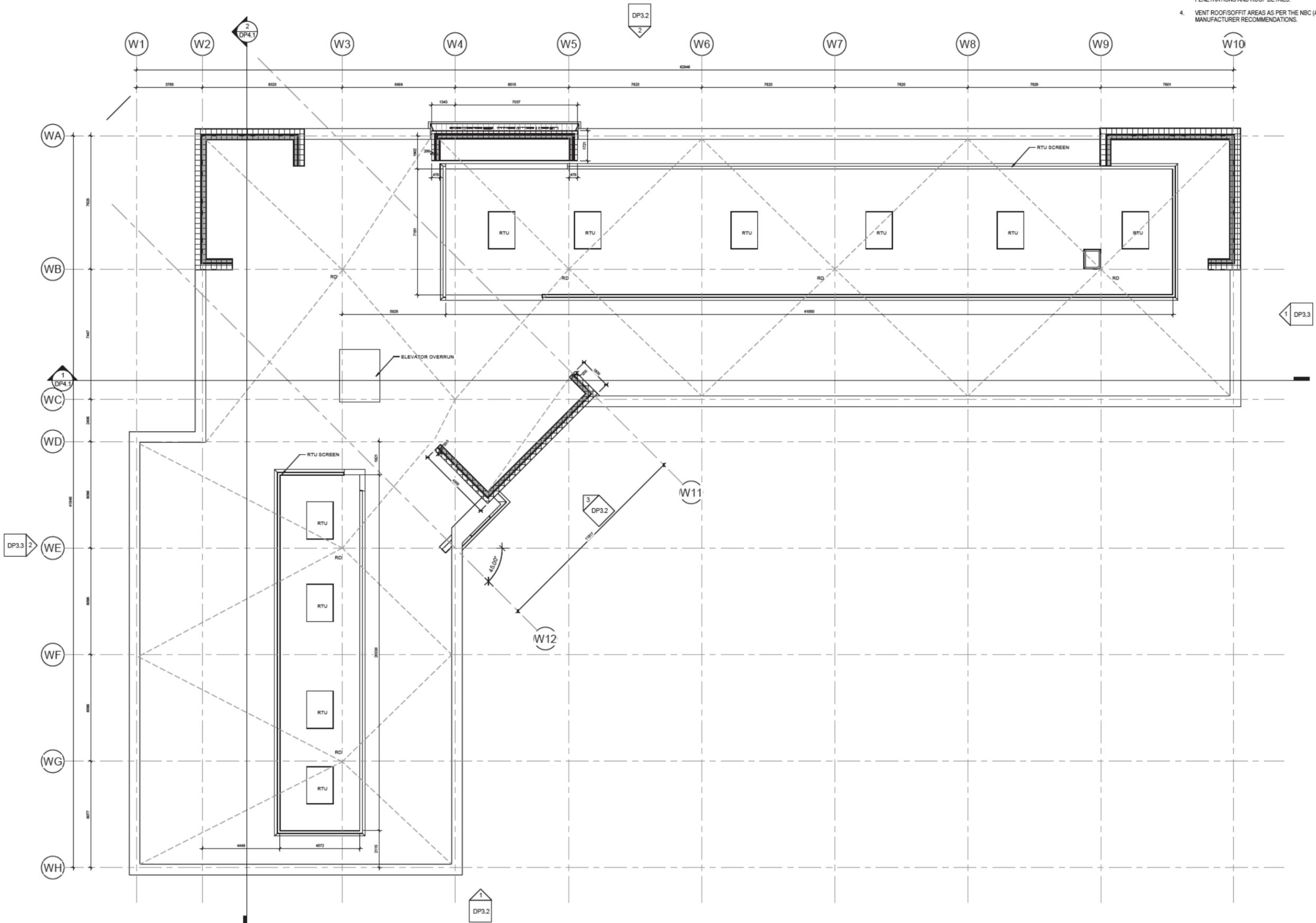


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ROOF PLAN - CBW

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1 CWB - T/O STEEL DECK
DP2.5 1:100

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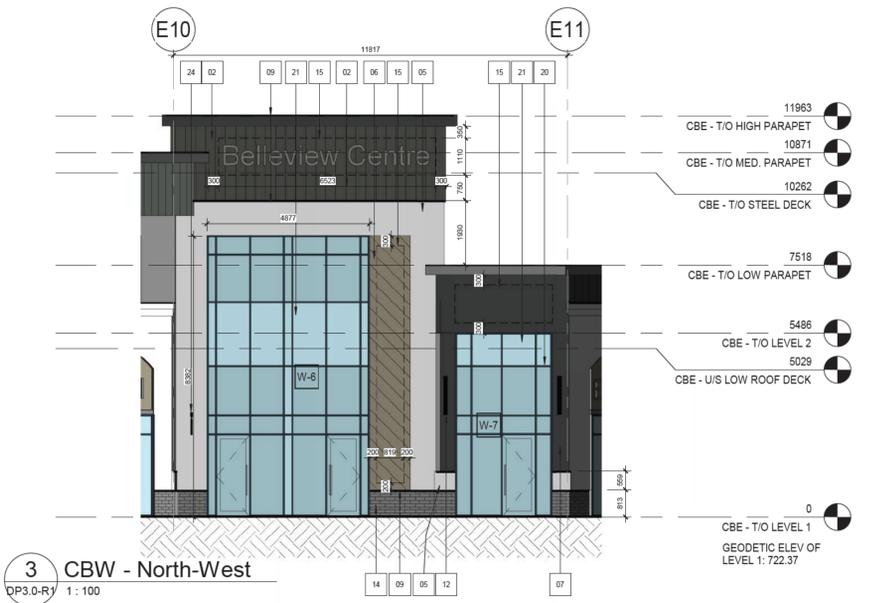
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1 SOUTH
DP3.0-R1 1: 100



2 NORTH
DP3.0-R1 1: 100



3 CBW - North-West
DP3.0-R1 1: 100

- GENERAL NOTES**
- REFER TO DOOR SCHEDULE FOR DOOR & WINDOW TYPES, AND SIZES.
 - 100'-0" IS THE DATUM ELEVATION AND CORRESPONDS TO THE GEODETIC ELEVATION OF THE LEVEL 1 FLOOR SLAB. SEE CIVIL FOR ADDITIONAL INFO.
 - CONTRACTOR TO VERIFY W/ ELEC DWGS FOR LIGHT NG LOCATIONS.
 - COORDINATE EXACT LOCATION OF MECHANICAL INTAKE AND EXHAUST TO AVOID CONFLICT W/ EXTERIOR TRIM, TYP.
 - CONTRACTOR TO PROVIDE 200X200MM COLOUR SAMPLES OF EXTERIOR MATERIALS TO ARCHITECT FOR CONFIRMATION



- COMMERCIAL ELEVATION KEYNOTE LEGEND:**
- | | |
|--|--|
| 01 EIFS - BLACK CORNICE TYPE 1 COLOUR 1 TEXTURE: SANDED / SABLE 303A | 11 EIFS REVEAL |
| 02 METAL PANELS - VICWEST COR78 COLOUR: BLACK | 12 LIGHT FIXTURE, REFER TO ELECTRICAL 10'-0" TO BOTTOM FROM GRADE OR 1' ABOVE EXIT DOOR |
| 03 EIFS - BLACK COLOUR 1 TEXTURE: SANDED / SABLE 303A | 13 SCUPPER |
| 04 EIFS - WHITE CORNICE TYPE 2 COLOUR 2 TEXTURE: SANDED / SABLE 303A | 14 MASONRY BLOCK HALF HIGH SMOOTH FACE FROM EXPOCRETE / ECHELON RUNN NG BOND W/ CONCAVED JOINT COLOUR: DARK CHARCOAL |
| 05 EIFS - WHITE COLOUR 2 TEXTURE: SANDED / SABLE 303A (PROVIDE IMPACT RESISTANT MESH AT ROOF DECK AND PLAY-AREA LOCATIONS) | 15 3/4" PLYWOOD SIGNAGE BACKING AS DIM'D PER ELEVATIONS |
| 06 METAL PANELS - VICWEST BELLARA PLANK COLOUR: WOODLIKE (WARM ROSEWOOD) NSTALLED @ 45 DEGREES ALT = LUX | 16 F RE DEPARTMENT CONNECTION |
| 07* EIFS - BLACK COLOUR 1 TEXTURE: SANDED / SABLE 303A ** OR BLACK METAL PANELS | 17 FD LOCK BOX 5'-0" FROM GRADE TO TOP |
| 08 EIFS - GREY COLOUR 3 TEXTURE: SANDED / SABLE 303A | 18 FDC - RED BEACON ABOVE THE FDC AT 8'-0" |
| 09 PREF NISHED ALUMINUM FLASHING / CAP FLASHING COLOUR: BLACK | 19 STEEL CHANNEL EYEBROW PAINTED DULUX COLOUR: BLACK |
| 10 INSULATED HOLLOW METAL DOOR W/ INSULATED PRESSED STEEL FRAME AND VIEW PANEL COLOUR: PAINT TO MATCH ADJACENT STUCCO COLOUR | 20 BLACK ANODIZED ALUM NUM WINDOW MULLIONS W/ SEALED UNIT GLAZING |
| | 21 SPANDREL PANEL ON 8MM ACI OPACI COAT BLACK MANUFACTURER: OPACI COAT COLOUR: BLACK |
| | 22 BLACK CANVAS AWNING ON 1"x1" EXTRUSION ALUM NUM FRAME (BLACK) PROVIDE 2"x8" BACK NG AT TOP AND BOTTOM OF THE HORIZONTAL LENGTH OF AWNING. |
| | 23 RTU SCREEN BEYOND - VICWEST COR78 COLOUR: BLACK |
| | 24 PROJECTING SIGNAGE - SEE SITE DETAILS HEIGHT 3.2M TO TOP OF BRACKET - CONTRACTOR TO INSTALL BACKING |
| | 25 VENEER GLASS WITH BLACK ANODIZED ALUM MULLION CAPS AND PRESSURE PLATE |

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BUILDING ELEVATIONS - CBE

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9 COLONIALE WAY, BEAUMONT, AB

BUILDING ELEVATIONS - CBE

Date: 2025-06-23
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 Scale: As indicated Rtn: 25-004
 Sheet No: DP3.1



1 CBE - East
DP3.1 1:100



2 CBE - West
DP3.1 1:100

- GENERAL NOTES**
- REFER TO DOOR SCHEDULE FOR DOOR & WINDOW TYPES, AND SIZES.
 - 100'-0" IS THE DATUM ELEVATION AND CORRESPONDS TO THE GEODETIC ELEVATION OF THE LEVEL 1 FLOOR SLAB. SEE CIVIL FOR ADDITIONAL INFO.
 - CONTRACTOR TO VERIFY W/ ELEC DWGS FOR LIGHTING LOCATIONS.
 - COORDINATE EXACT LOCATION OF MECHANICAL INTAKE AND EXHAUST TO AVOID CONFLICT W/ EXTERIOR TRIM, TYP.

- COMMERCIAL ELEVATION KEYNOTE LEGEND:**
- | | |
|--|--|
| 01 EIFS - BLACK CORNICE TYPE 1 COLOUR 1 TEXTURE: SANDED / SABLE 303A | 11 EIFS REVEAL |
| 02 METAL PANELS - VICWEST COR78 COLOUR: BLACK | 12 LIGHT FIXTURE. REFER TO ELECTRICAL 10'-0" TO BOTTOM FROM GRADE OR 1' ABOVE EXIT DOOR |
| 03 EIFS - BLACK COLOUR 1 TEXTURE: SANDED / SABLE 303A | 13 SCUPPER |
| 04 EIFS - WHITE CORNICE TYPE 2 COLOUR 2 TEXTURE: SANDED / SABLE 303A | 14 MASONRY BLOCK HALF HIGH SMOOTH FACE FROM EXPOCRETE RUNNING BOND W/ CONCAVED JOINT COLOUR: DARK CHARCOAL |
| 05 EIFS - WHITE COLOUR 2 TEXTURE: SANDED / SABLE 303A (PROVIDE IMPACT RESISTANT MESH AT ROOF DECK AND PLAY-AREA LOCATIONS) | 15 3/4" PLYWOOD SIGNAGE BACKING AS DIMD PER ELEVATIONS |
| 06 METAL PANELS - VICWEST BELLARA PLANK COLOUR: WOODLIKE (WARM ROESWOOD) INSTALLED @ 45 DEGREES ALT = LUX | 16 FIRE DEPARTMENT CONNECTION |
| 07** EIFS - BLACK COLOUR 1 TEXTURE: SANDED / SABLE 303A ** OR BLACK METAL PANELS | 17 FD LOCK BOX 5'-0" FROM GRADE TO TOP |
| 08 EIFS - TAN COLOUR 3 TEXTURE: SANDED / SABLE 303A | 18 FDC - RED BEACON ABOVE THE FDC AT 8'-0" |
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| | 23 RTU SCREEN BEYOND - VICWEST COR78 COLOUR: BLACK |



1 CBW - South
DP3.2-R1 1:100

- 11963 CBW - T/O HIGH PARAPET
- 10871 T/O MED. PARAPET - CBW
- 10262 CWB - T/O STEEL DECK
- 5486 CWB - T/O LEVEL 2
- 0 CBW - T/O LEVEL 1
GEODEIC ELEV OF LEVEL 1: 722.95'



2 CBW - North
DP3.2-R1 1:100

- 11963 CBW - T/O HIGH PARAPET
- 10871 T/O MED. PARAPET - CBW
- 10262 CWB - T/O STEEL DECK
- 5486 CWB - T/O LEVEL 2
- 0 CBW - T/O LEVEL 1
GEODEIC ELEV OF LEVEL 1: 722.95'



3 CBW - North-East
DP3.2-R1 1:100

- 11963 CBW - T/O HIGH PARAPET
- 10871 T/O MED. PARAPET - CBW
- 10262 CWB - T/O STEEL DECK
- 5486 CWB - T/O LEVEL 2
- 0 CBW - T/O LEVEL 1
GEODEIC ELEV OF LEVEL 1: 722.95'

GENERAL NOTES

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COMMERCIAL ELEVATION KEYNOTE LEGEND:

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| 04 EIFS - WHITE CORNICE TYPE 2 COLOUR 2 TEXTURE: SANDED / SABLE 303A | 14 MASONRY BLOCK HALF HIGH SMOOTH FACE FROM EXPOCRETE / ECHELON RUNNING BOND W/ CONCAVED JOINT COLOUR: DARK CHARCOAL |
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Orig Thomas, RFP, MCP
Development Authority

BEAUMONT

Date: January 5, 2026

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COLONIALE WAY
COMMERCIAL
9 COLONIALE WAY, BEAUMONT, AB

BUILDING ELEVATIONS - CBW

Date: 2025-10-27

Drawn by: SK Checked by: SK

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1 CBW - East
DP3.3 1:100



2 CBW - West
DP3.3 1:100

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- COMMERCIAL ELEVATION KEYNOTE LEGEND:**
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| 04 EIFS - WHITE CORNICE TYPE 2 COLOUR 2 TEXTURE: SANDED / SABLE 303A | 14 MASONRY BLOCK HALF HIGH SMOOTH FACE FROM EXPOCRETE RUNNING BOND W/ CONCAVED JOINT COLOUR: DARK CHARCOAL |
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| | 23 RTU SCREEN BEYOND - VICWEST COR78 COLOUR: BLACK |

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COLONIAL WAY COMMERCIAL
9 COLONIAL WAY, BEAUMONT, AB

BUILDING ELEVATIONS - CBW

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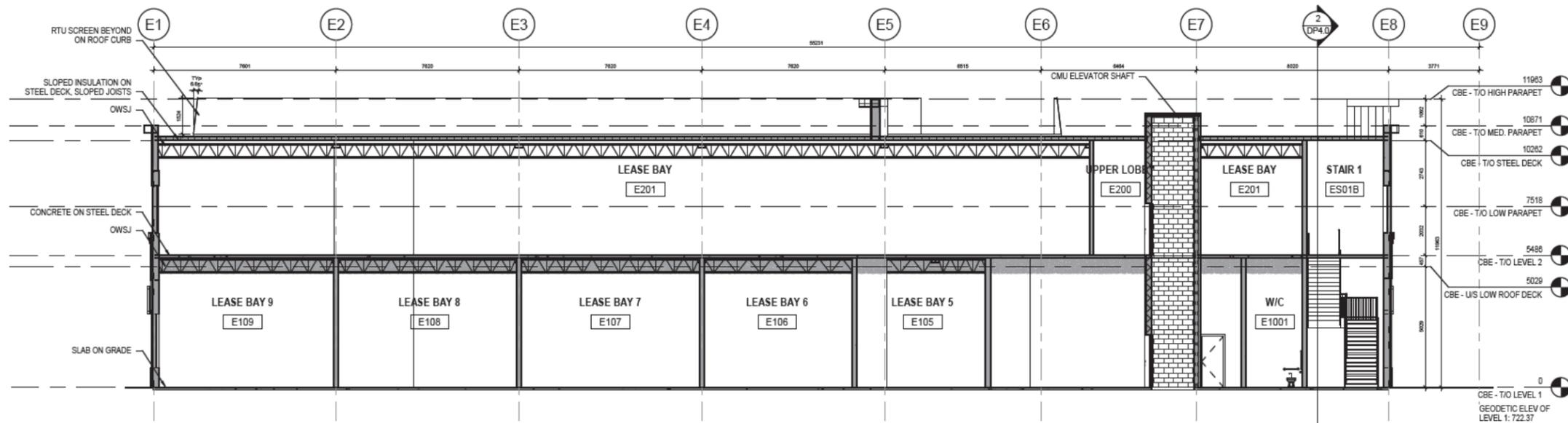
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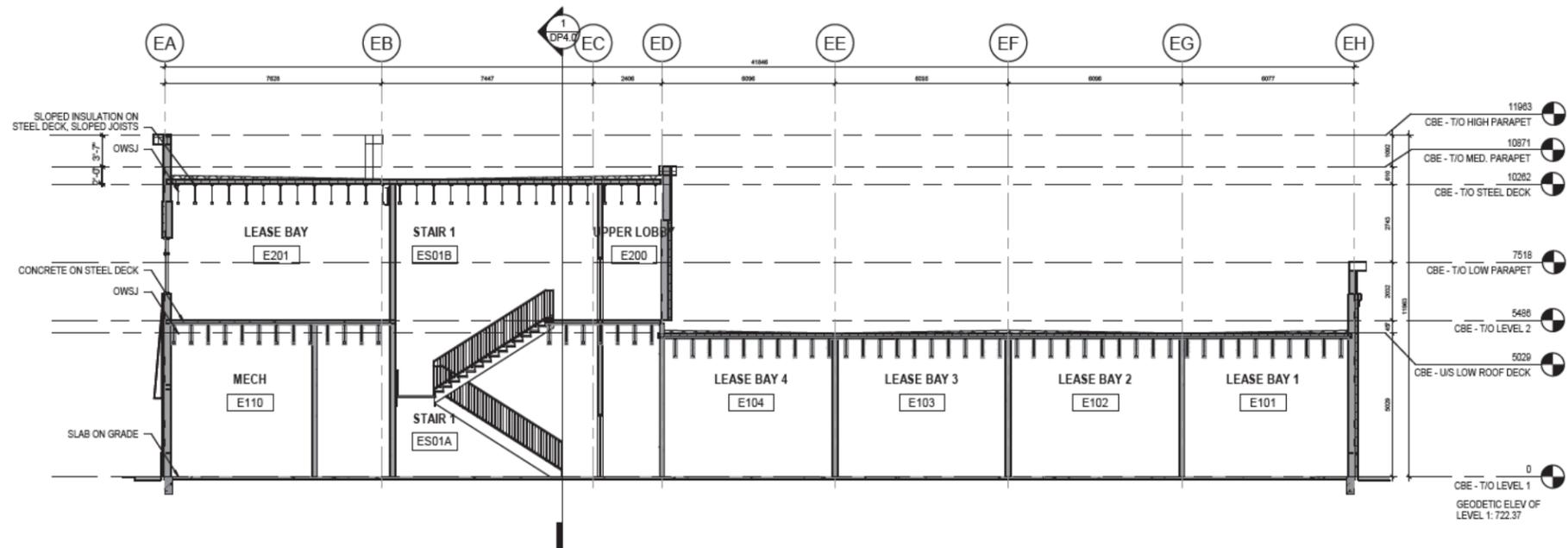
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Only The
Developer
Date: **January 5, 2026**



1 CBE - SECTION 1
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2 CBE - SECTION 2
DP4.0 1:100

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Issue / Permit Stamp

COLONIAL WAY COMMERCIAL
9 COLONIAL WAY, BEAUMONT, AB

| BUILDING SECTIONS - CBE | |
|-------------------------|------------|
| Date | 2025-06-23 |
| Drawn by | SK |
| Scale | 1:100 |
| Sheet No. | 25-004 |
| DP4.0 | |

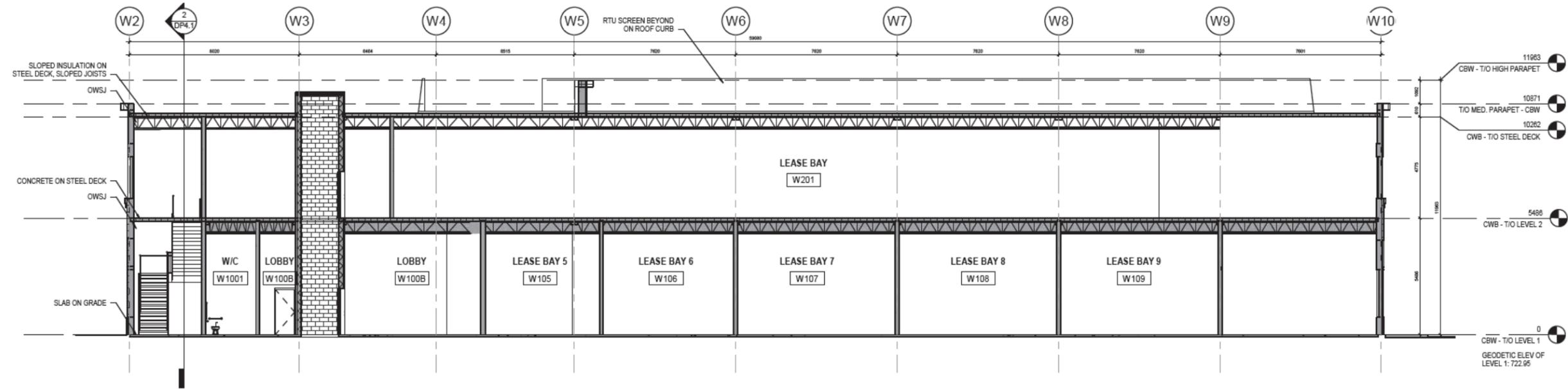
GENERAL NOTES

1. ALL STRUCTURAL MEMBERS / COMPONENTS / CONNECTIONS ARE TO BE VERIFIED, APPROVED & STAMPED BY A STRUCTURAL ENGINEER PRIOR TO CONSTRUCTION.
2. REFER TO GEOTECH REPORT FOR WEEPING TILE REQ'S.

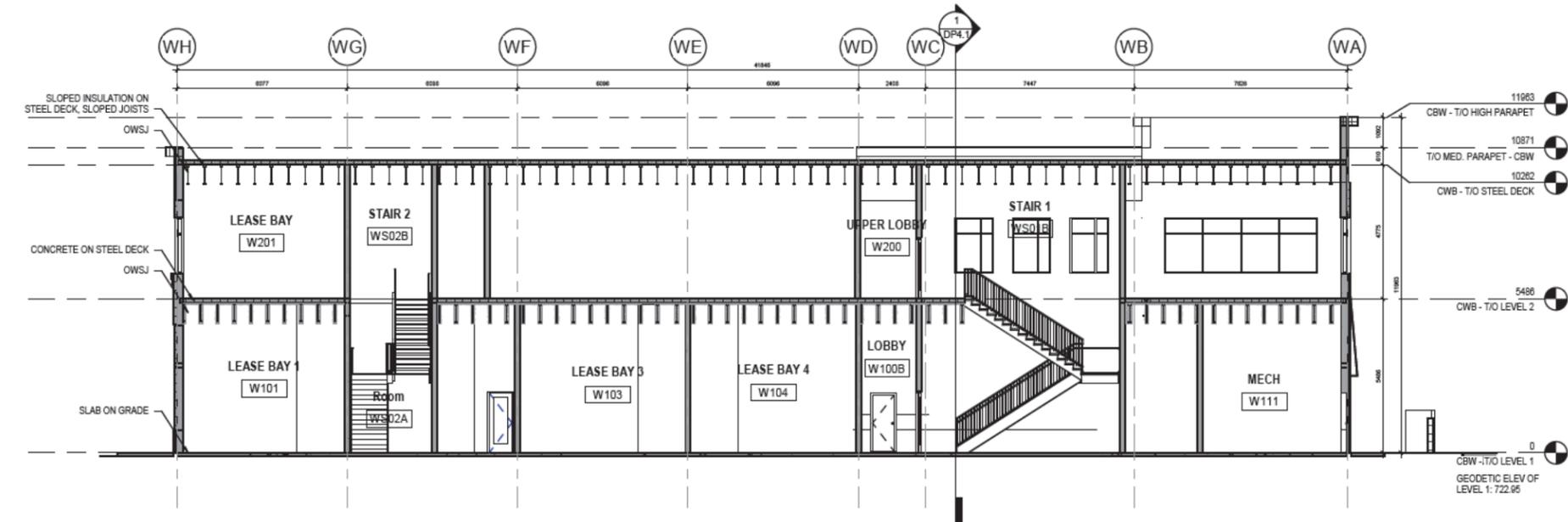


| DRAWING ISSUE / REVISION | | |
|--------------------------|--------------|---------------|
| No. | Issued For | Date |
| 1 | ISSUE FOR DP | JUNE 23, 2025 |
| | | |
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1 CBW - SECTION 3
DP4.1 1:100



2 CBW - SECTION 4
DP4.1 1:100

ISSUE FOR DEVELOPMENT PERMIT
NOT FOR CONSTRUCTION

Draw / Permit Stamp

COLONIALE WAY COMMERCIAL
9 COLONIALE WAY, BEAUMONT, AB

| BUILDING SECTIONS - CBW | |
|-------------------------|--------------|
| Date | 2025-06-23 |
| Drawn by | SK |
| Checked by | SK |
| Scale | As indicated |
| Sheet No. | 25-004 |
| DP4.1 | |

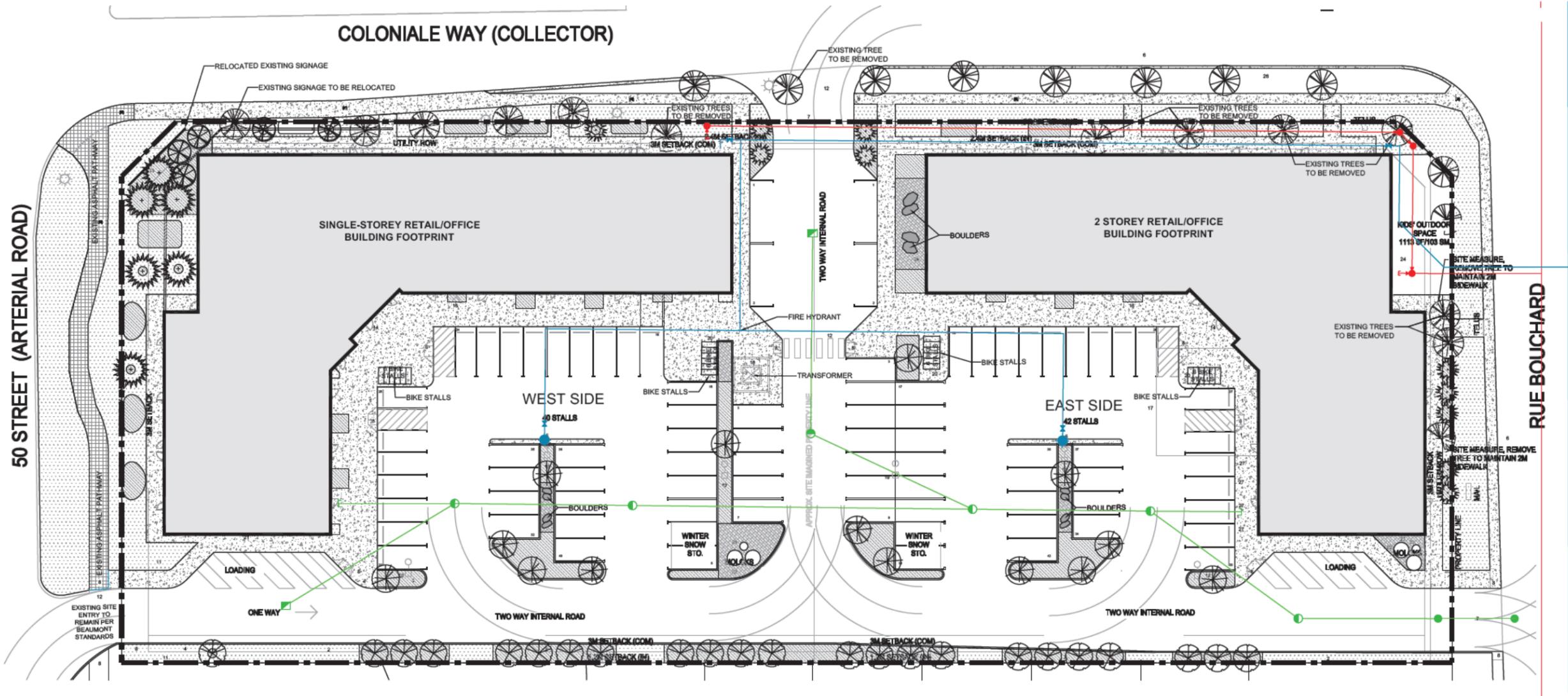
CLIENT :
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PROJECT :
**9 COLONIALE WAY
 BEAUMONT, AB**

ARCHITECT :
 .

**ISSUED FOR DP
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STAMP:



1 SITE PLAN
 1:500

UTILITIES

- WATER
- SANITARY
- DRAINAGE

LEGEND

- WOOD MULCH
- GRAVEL MULCH
- SOD
- CONCRETE
- PROPERTY LINE
- PROPOSED TREES
- PROPOSED SHRUBS & GRASSES
- EXISTING TREES

NOTES:

- QUANTITIES GIVEN ARE FOR INFORMATION ONLY.
- ALL PLANTS ARE TO BE HEALTHY, FULL, BALANCED, AND EXCEPTIONALLY HEAVY. ALL PLANT MATERIAL SHALL MEET THE HORTICULTURAL STANDARDS OF THE MOST CURRENT EDITION OF THE "GUIDE SPECIFICATIONS FOR NURSERY STOCK" PRODUCED BY THE CANADIAN NURSERY TRADE ASSOCIATION.
- PROVIDE TURF IN ALL DISTURBED AREAS NOT OTHERWISE PLANTED OR PAVED.
- PROVIDE GREY SAND STONE WOOD MULCH AROUND THE TREES AS SHOWN IN THE PLAN AS WELL AS IN DETAILS.
- THE EXISTING TREES IN THE CITY OWNED LANDS TO BE PROTECTED.

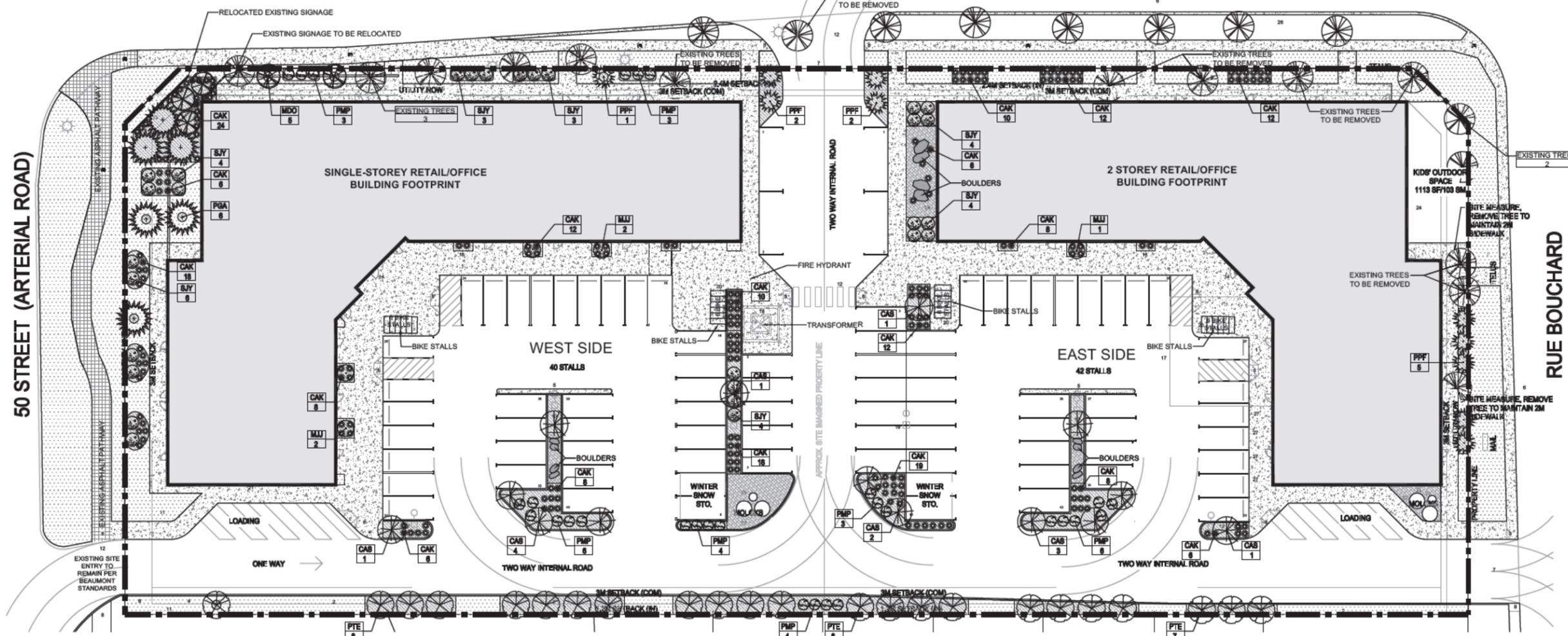
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| R2 | 2025.11.05 |
| R1 | 2025.09.23 |
| R0 | 2025.06.25 |

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| | SITE PLAN | |

Project No. : D24-
 Scale : 1:500
 Drawn By : PS
 Checked By : DD/AD
 Date : -

Sheet No. L-101

COLONIALE WAY (COLLECTOR)



GREEN SPACE ALLIANCE
 Suite 205, Sylbert Building,
 10132 - 105 St. NW
 Edmonton AB T5J1C9
 T: +1 780 710 0035

CLIENT :

PROJECT :
9 COLONIALE WAY
 BEAUMONT, AB

ARCHITECT :

ISSUED FOR DP
 NOT FOR CONSTRUCTION

STAMP:

APPROVED
 Development Authority
 Date: **January 5, 2026**

Please refer to Development Permit condition #6 regarding tree plantings adjacent to boulevard.

2 PLANTING PLAN
 1:500

PLANT CALCULATION

| TREES REQUIRED | EXISTING TREES | TREES PROVIDED | TOTAL TREES | SHRUBS REQUIRED | SHRUBS PROVIDED | GRASSES PROVIDED |
|----------------|----------------|----------------|-------------|-----------------|-----------------|------------------|
| 38 | 5 | 63 | 68 | 46 | 57 | 203 |

PLANT LIST

| KEY | QTY | BOTANICAL NAME | COMMON NAME | SIZE | TYPE |
|--------------------------|-----|----------------------------------|---------------------------------|-----------------|-------------|
| DECIDUOUS TREES | | | | | |
| MDO | 5 | Malus 'DurLeo' | Gladiator Flowering Crab | 5.5 m X 2.7 m | FULL, DENSE |
| MJJ | 5 | Malus 'Jefspire' | Purple Spire Columnar Crabapple | 4.57 m X 1.8 m | FULL, DENSE |
| CAS | 14 | Caragana arborescens 'Sutheland' | Sutherland Caragana | 6.09 m X 3.04 m | FULL, DENSE |
| PTE | 23 | Populus tremula 'Erecta' | Swedish Aspen | 15.2 m X 3.0 m | FULL, DENSE |
| CONIFEROUS TREES | | | | | |
| PGA | 6 | Picea glauca | White Spruce | 18.0 m X 6.0 m | FULL, DENSE |
| PPF | 10 | Picea pungens 'Fastigiata' | Columnar Blue Colorado Spruce | 6.0 m X 2.4 m | FULL, DENSE |
| DECIDUOUS SHRUBS | | | | | |
| SJY | 28 | Syringa 'Jeflady' | Little Lady Lilac | 1.5 m X 1.5 m | FULL, DENSE |
| CONIFEROUS SHRUBS | | | | | |
| PMP | 29 | Pinus mugo var. pumilio | Dwarf Mugo Pine | 1.2 m X 1.2 m | FULL, DENSE |
| GRASS | | | | | |
| CAK | 203 | Calamagrostis Acutiflora | Karl Foerster Reed Grass | 1.2 m X 0.8 m | FULL, DENSE |

COST ESTIMATE FOR PLANTING

| BOTANICAL NAME | COMMON NAME | QTY | COST PER UNIT | TOTAL COST |
|----------------------------------|---------------------------------|------------------|---------------|------------|
| DECIDUOUS TREES | | | | |
| Malus 'DurLeo' | Gladiator Flowering Crab | 5 | \$500 | \$2500 |
| Malus 'Jefspire' | Purple Spire Columnar Crabapple | 5 | \$500 | \$2500 |
| Caragana arborescens 'Sutheland' | Sutherland Caragana | 14 | \$500 | \$7000 |
| Populus tremula 'Erecta' | Swedish Aspen | 23 | \$500 | \$11500 |
| CONIFEROUS TREES | | | | |
| Picea glauca | White Spruce | 6 | \$600 | \$3600 |
| Picea pungens 'Fastigiata' | Columnar Blue Colorado Spruce | 10 | \$600 | \$6000 |
| DECIDUOUS SHRUBS | | | | |
| Syringa 'Jeflady' | Little Lady Lilac | 28 | \$80 | \$2240 |
| CONIFEROUS SHRUBS | | | | |
| Pinus mugo var. pumilio | Dwarf Mugo Pine | 29 | \$80 | \$2320 |
| GRASS | | | | |
| Calamagrostis Acutiflora | Karl Foerster Reed Grass | 203 | \$80 | \$16240 |
| | | Trees Subtotal | | \$33100 |
| | | Shrubs Subtotal | | \$4560 |
| | | Grasses Subtotal | | \$16240 |
| | | Sub Total | | \$53900 |

LEGEND

- WOOD MULCH
- GRAVEL MULCH
- SOD
- CONCRETE
- PROPERTY LINE
- PROPOSED TREES
- PROPOSED SHRUBS & GRASSES
- EXISTING TREES

COST ESTIMATE FOR SOFTSCAPING

| SUPPLY & INSTALLATION OF | AREA Sq.m | DEPTH m | VOLUME Cubic m | COST | PER UNIT | TOTAL |
|--------------------------|-----------|---------|----------------|-------------------|----------|------------|
| SOD | 422.94 | - | - | \$30 | Sq.m | \$12688.2 |
| TOP SOIL FOR SOD | 422.94 | 0.20m | 84.59 | \$75 | Cubic m | \$6344.25 |
| WOOD MULCH | 534.82 | 0.10m | 53.48 | \$90 | Cubic m | \$4813.2 |
| 40mm ROCK/GRAVEL MULCH | 87.91 | 0.075m | 6.6 | \$140 | Cubic m | \$924 |
| PLANTING BED | 622.73 | 0.45m | 280.23 | \$75 | Cubic m | \$21017.25 |
| | | | | Softscaping Total | | \$45786.9 |

| SIZE OF TREES AND SHRUBS | |
|--------------------------|-------------------------------------|
| DECIDUOUS TREES | Minimum 60mm CAL. |
| CONIFEROUS TREES | Minimum 2.4m Height |
| DECIDUOUS SHRUBS | Minimum 400mm Height (5 gallon pot) |
| CONIFEROUS SHRUBS | Minimum 300mm Spread (5 gallon pot) |

TOTAL COST FOR PLANTATION & SOFTSCAPING = \$ 99686.9
 GST (5%) = \$ 4984.35
 TOTAL LANDSCAPE COST (100% OF LANDSCAPING COST+ GST) = \$ 104671.25

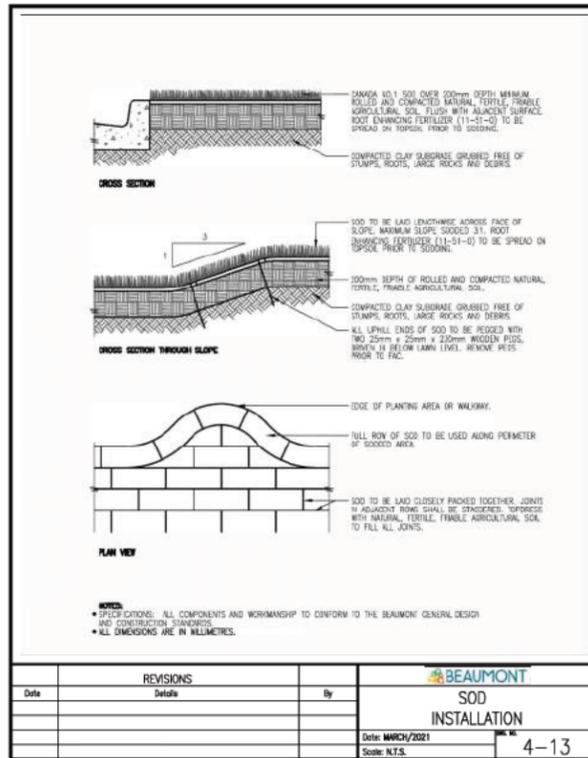
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| R1 | 2025.09.23 |
| R0 | 2025.06.25 |

| No. | Description | Date |
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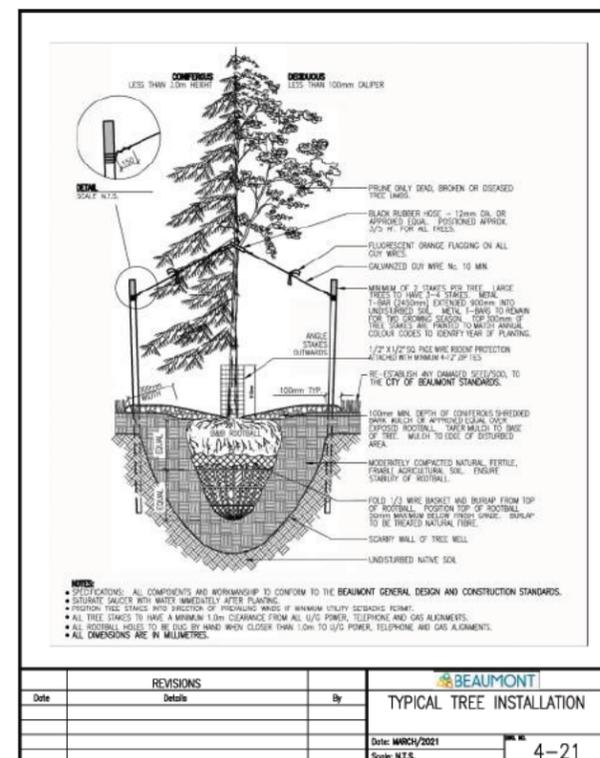
PLANTING PLAN

Project No. : D24-
 Scale : 1:500
 Drawn By : PS
 Checked By : DD/AD
 Date : -

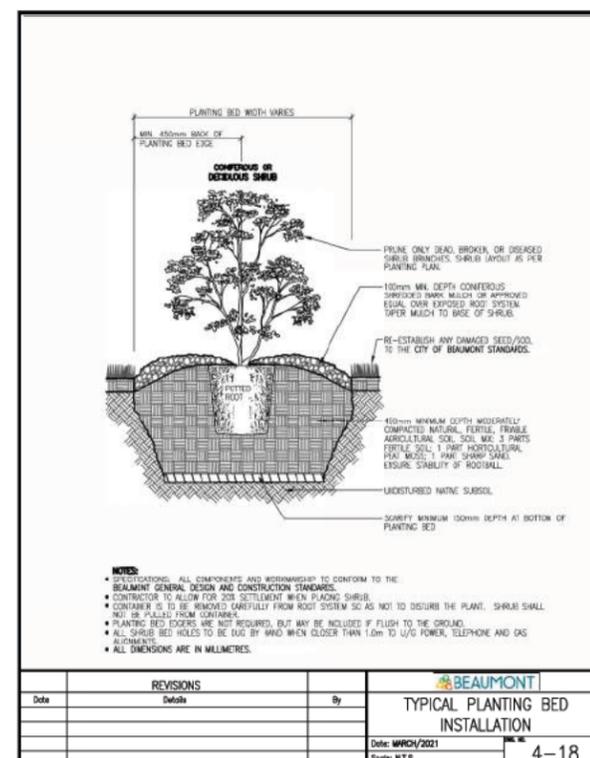
Sheet No. L-102



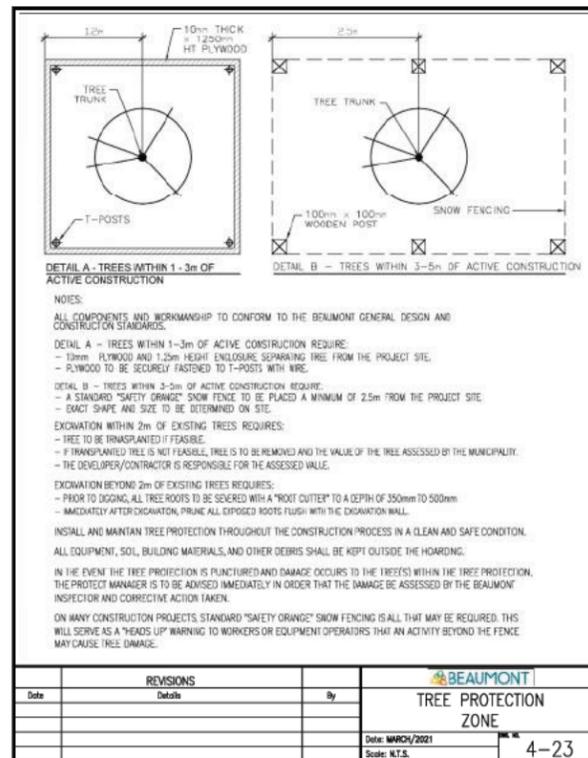
1 SOD INSTALLATION
NTS



2 TYPICAL TREE INSTALLATION
NTS



3 TYPICAL PLANTING BED INSTALLATION
NTS



4 TREE PROTECTION ZONE
NTS

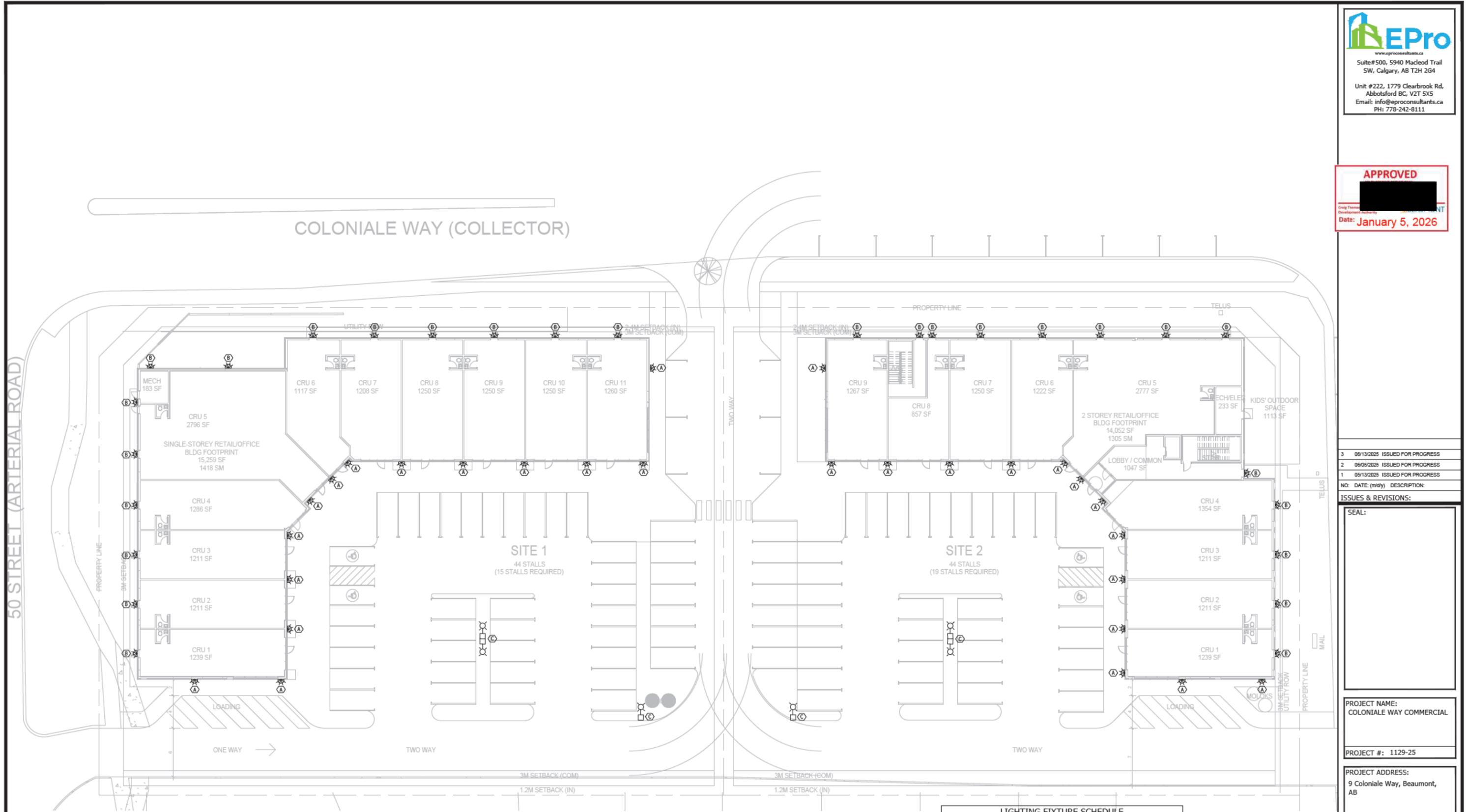


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| R2 | 2025.11.05 |
| R1 | 2025.09.23 |
| R0 | 2025.06.25 |

| No. | Description | Date |
|------------------------|-------------|------|
| TYPICAL DETAILS | | |

Project No. : D24-
Scale : NTS
Drawn By : PS
Checked By : DD/AD
Date : -

APPROVED
[Redacted Signature]
Date: **January 5, 2026**



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|---|------------|---------------------|
| 3 | 06/13/2025 | ISSUED FOR PROGRESS |
| 2 | 05/05/2025 | ISSUED FOR PROGRESS |
| 1 | 05/13/2025 | ISSUED FOR PROGRESS |

NO: DATE (m/d/y) DESCRIPTION:

ISSUES & REVISIONS:

SEAL:

PROJECT NAME:
COLONIALE WAY COMMERCIAL

PROJECT #: 1129-25

PROJECT ADDRESS:
9 Coloniale Way, Beaumont, AB

DATE: MAY-12-2025

DSN: TS **CHK:** JK

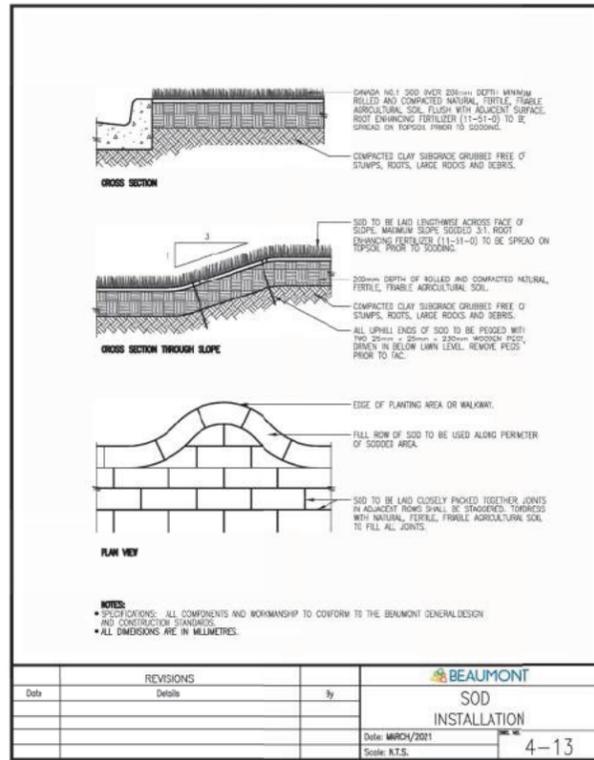
SCALE: AS NOTED
DRAWING TITLE:
SITE PLAN

DRAWING #: E-1.1

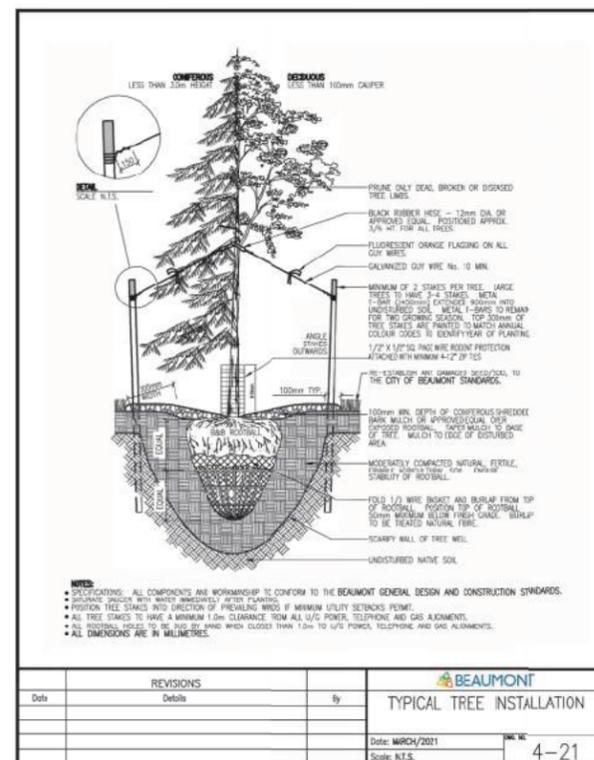
| LIGHTING FIXTURE SCHEDULE | | |
|--|-----------|------------------------------------|
| FIXTURE TYPE | LAMP | MOUNTING |
| Ⓐ LITHONIA #WIDG2 LED P2 T9H 40K 80CRI | 19.0W LED | WALL MOUNTED +12' AFG |
| Ⓑ LITHONIA #WIDG2 LED P1 T2H 40K 80CRI | 11.2W LED | WALL MOUNTED +12' AFG |
| Ⓒ LITHONIA #D5X1 LED P1 40K T4H MVOLT | 50.9W LED | 17' POLE LIGHT AT 3' CONCRETE BASE |

NOTES:
FIXTURES TO BE AS SPECIFIED OR APPROVED EQUAL.

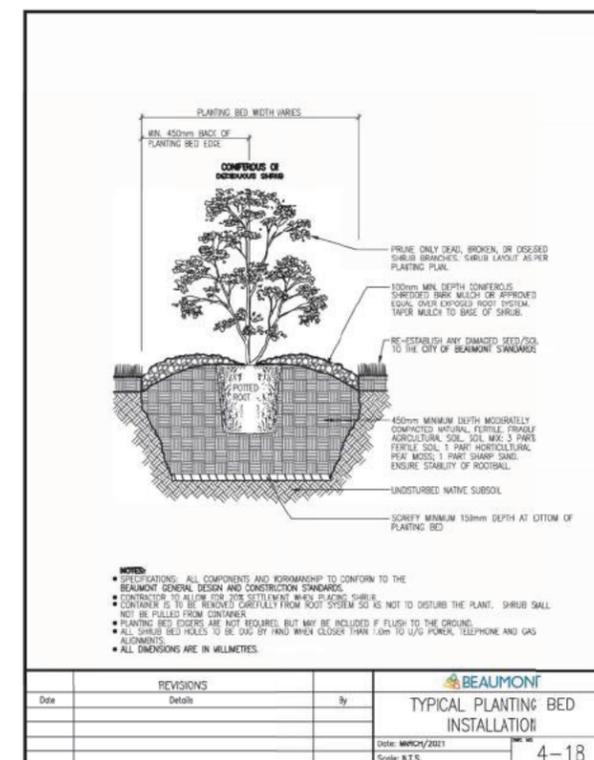
SITE PLAN
SCALE: 1:200



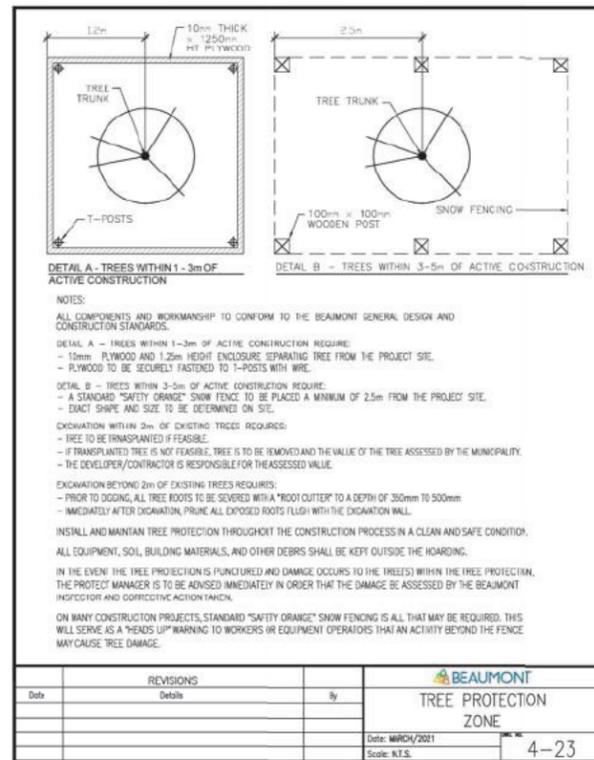
1 SOD INSTALLATION
NTS



2 TYPICAL TREE INSTALLATION
NTS



3 TYPICAL PLANTING BED INSTALLATION
NTS



4 TREE PROTECTION ZONE
NTS



CLIENT :

PROJECT :
9 COLONIALE WAY
BEAUMONT, AB

ARCHITECT :

ISSUED FOR DP
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STAMP:

RO 2025.06.19

| No. | Description | Date |
|-----|-------------|------|
| | | |

TYPICAL DETAILS

Project No. : D24-
Scale : NTS
Drawn By : PS
Checked By : DD/AD
Date : -

Sheet No. L-103

OUR COMPLETE COMMUNITY

MUNICIPAL DEVELOPMENT PLAN



ACKNOWLEDGEMENTS

Our Complete Community was created from the efforts of Beaumont staff and Stantec Consulting. The project team would like to thank the stakeholders who have dedicated their time and energy into the creation of the plan by providing feedback at various workshops, open houses, interviews, and other engagement events.

Acknowledgement would also like to be given to Beaumont's Council and the Working Group who provided insight throughout the project.

| | | |
|----------------------|--------------------|---------------------|
| BEAUMONT COUNCIL: | Mayor John Stewart | Martin Stout |
| | Perry Hendriks | Sam Munckhof-Swain |
| | Kathy Barnhart | Steven vanNieuwkerk |
| | Bill Daneluik | |

| | | |
|---------------------------------|----------------------|--------------------|
| PAST COUNCIL (2013-2017): | Mayor Camille Bérubé | Bruce LeCren |
| | Kathy Barnhart | Bill McNamara |
| | Kerri Bauer | Louise White-Gibbs |
| | Perry Hendriks | |

| | |
|------------------------------------|---|
| WORKING GROUP: | Eleanor Mohammed, General Manager, Integrated Growth & Infrastructure |
| | Kerry Hilts, General Manager, Community & Protective Services |
| | Kendra Raymond, Director, Integrated Growth |
| | Kathy Lewin, Director, Infrastructure |
| | Rob Mackin, Director, Economic Development |
| | Joannes Wong, Manager, Long Range Planning |
| | Tyler Tymchyshyn, Manager, Engineering |
| | Joanne Dargis, Manager, Current Planning |
| | Shiva Kumar, Manager, Media and Communications |
| | Daya Ranasinghe, Geomatics Coordinator |
| | Alyssa Carson, Environmental Sustainability Coordinator |
| | Lisa Drury, Long Range Planner |
| | Ruibin Li, Long Range Planner |
| Aleshia Kwasny, Current Planner | |
| Paxton Porter, Planning Technician | |

PHOTO
CREDITS:

Several of the photographs used in the document are the creative works of residents of the community who entered a photo contest as part of the project. Their photos demonstrate the varied landscapes, wildlife, and built form that residents of Beaumont can enjoy. Thank you for all the submissions that have helped to make this plan interesting and visually appealing. Photos featured in the plan are credited to:

Colton Nelson (Cover page, Pg. 1, 11, 45, 69, and back cover)
Elaine Grandon (Pg. 8, 13, 49, and back cover)
Tracy Galenza (Pg. 1, 5, 12, 17, , 104, and back cover)
Joanne Morrison (Pg. 18)
Monica Helgoth (Pg. 14, 55, and back cover)

Note:

Acknowledgement would also like to be given to former Working Group members who provided insight throughout the project:

Dave Dmytryshyn
Ilona Molnar
Bert Roach

Nick Nilsen
Sadie Miller

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1

Introduction



2.5 Our Complete Community: A Vision For Beaumont

Beaumont's community-built vision guides our everyday plans and actions. The policies, goals, and objectives of *Our Complete Community* have been developed to achieve this vision.

"Together, we make life better in Beaumont.

As a prosperous, vibrant, healthy, family-oriented community, Beaumont welcomes diversity, nurtures business, promotes excellence, and is environmentally conscious, while celebrating its Indigenous, agricultural, and French heritage."

Six overarching policy theme areas for *Our Complete Community* were identified through the public process.

These theme areas are:

Healthy Vibrant Community

Important policy elements that together create an attractive, desirable community that provides a high quality of life for residents of all ages, which include arts and culture, placemaking, history, and social equity.

Responsible Development

Policies guiding future development of lands in Beaumont.

Effective Movement of People and Goods

Policies related to the transportation system with a focus on safe, inclusive, and multi-modal options for residents and businesses.

Environmental Stewardship

Policies that ensure the protection and expansion of Beaumont's environment to ensure future generations can enjoy Beaumont's resources.

Economic Strength

Key components for successful growth and attracting diverse and innovative local employment opportunities.

Safety and Emergency Management

Safety and Emergency Management is included in the plan to ensure the safety, health, and welfare of Beaumont's residents.

These themes are overlapping and interconnected; they build upon each other and support each other. You will see different elements of each theme incorporated in other sections to reinforce important concepts and ideas. These themes should be applied holistically as they work together to build a complete community.

2.6 Generalized Land Use Concept

Map 9 – Land Use Concept, details the land use policy areas to guide the long-term growth and development of Beaumont. Ten generalized character areas have been identified based on current development and the desired future development as established through the *Our Complete Community* vision and key goals. The land use concept has been designed to align

with the growth directives in the Edmonton Metropolitan Region Growth Plan to appropriately intensify Beaumont's built-up and planned areas, in addition to ensuring that Beaumont's greenfield areas achieve a density of 35 dwelling units per net residential hectare along with supporting transit, employment, urban agriculture, and affordable housing opportunities.

RESIDENTIAL AREAS

Established Residential

The Established Residential character area, identified on **Map 9 – Land Use Concept**, is also considered the built-up and planned area of Beaumont as per the Edmonton Metropolitan Region Growth Plan. The established residential area encompasses all existing residential neighbourhoods within the community that have an existing approved Outline Plan or Area

Structure Plan. Policies for the Established Residential policy area are intended to support maintenance or enhancement of the residential neighbourhood while providing opportunities for infill, redevelopment, and intensification. The Established Residential area has an intensification goal of 10% in accordance with the Edmonton Metropolitan Region Growth Plan.

Future Residential

The Future Residential character area identified on **Map 9 – Land Use Concept** is contained within the greenfield area of Beaumont identified on **Map 8 – Metropolitan Structure Components** in accordance with the Edmonton Metropolitan Region Growth Plan. The Future Residential policy area applies to areas intended for residential neighbourhood development that do not have an existing approved Outline Plan or Area Structure Plan. The

Future Residential area is primarily intended for a broad spectrum of residential housing types to meet the needs of residents at all ages and stages of life. Future Residential areas may include complementary uses such as schools, churches, recreational facilities, parks, open space, trails, utilities, and neighbourhood level commercial opportunities including mixed-use developments.

LOCAL EMPLOYMENT AREAS

Commercial

Commercial uses for areas identified on **Map 9 – Land Use Concept** include concentrations of retail, office, and entertainment uses, including large-format stores that attract a community-wide customer base. The policy area also includes a broad range of commercial uses that rely on

highway and major corridor access and exposure, such as auto services, sales and gas stations, hotels, restaurants, and other hospitality services. These areas are designed to take advantage of key transportation routes and integrate highway commercial opportunities.

Light Industrial

The Light Industrial character area identified on **Map 9 – Land Use Concept**, is intended to accommodate a range of light uses such as warehousing, equipment and material storage, manufacturing, and general light industrial operations that may require some limited outside storage. Screening standards will be implemented through the Land Use Bylaw. The Light Industrial area will maintain separation from non-industrial

land uses through the use of roadways, landscaped berms, or buffers to minimize land use conflicts and mitigate potential negative impacts such as visual, dust, odour, noise, and other esthetic impacts on surrounding users. Limited commercial uses like gas stations, convenience retail, and restaurants may be permitted where appropriate. This area also includes Beaumont's Operations Facility and is designated for a future transit centre.

Business Park

The Business Park character area identified on **Map 9 – Land Use Concept**, is intended for development of a compatible mix of employment and commercial and light industrial uses in a campus like setting such as offices, warehouses, and light manufacturing where all activities are carried out entirely within the confines of the building. The Business Park policy area

has higher design, landscaping, and signage requirements than typical light industrial uses that will be established and regulated through the Land Use Bylaw. These areas will also accommodate external public spaces and be at higher intensities to incorporate multiple stories of development for a variety of office and other employment opportunities.

Community Commercial/Employment Centre

Community Commercial/Employment Centres identified on **Map 9 – Land Use Concept**, are not intended to accommodate large-format retail uses but are designed to accommodate a mix of retail, offices and institutional uses that may also include multi-unit residential integrated with other uses. Residential components of Community Commercial/Employment Centres will be predominantly medium to high-density residential development. Community Commercial/Employment Centres are intended to be a focal point and meeting place for the community by

providing an attractive pedestrian-friendly public realm and gathering spaces that include patios and plazas. Pedestrian and cyclist connections will be provided between commercial and residential areas. Parking will be located behind buildings or underground to enable dense urban form. Community Commercial/Employment Centres will be located along arterial roads and will incorporate transit opportunities as part of the Centre. The principles of Transit Oriented Development will be implemented as part of Community Commercial/Employment Centres.

FUTURE GROWTH AREA

Future Growth area includes lands north of Township Road 510 between Range Road 241 and Range Road 243. This area is part of an agreed upon study area within the Intermunicipal Planning Framework Agreement signed by the Mayors of the City of Edmonton, Leduc County, and Beaumont for collaborative joint planning (land use, servicing

and transportation, as well as cost and benefit sharing) that recognizes that by working together, more sustainable and liveable communities in the Edmonton Metropolitan Region can be achieved.

CENTRE-VILLE

Centre-Ville is the historic and cultural heart of Beaumont and the Urban Centre of the community as per the Edmonton Metropolitan Region Growth Plan. Centre-Ville identified on **Map 9 – Land Use Concept**, will continue to be developed and revived with the goal of making it the major business, social, cultural, and entertainment focus of the community with a distinct French flair. This will be achieved through application of the Beaumont Urban Design Guidelines and development of a public realm that includes a unique and distinctive

streetscape with a focus on pedestrian-orientated mixed-use developments where people can live and work near daily amenities in close proximity to major transit facilities. Centre-Ville is intended to accommodate mixed-use development at higher intensities with development striving to meet an aspirational density target of 100 dwelling units per net residential hectare over the horizon of *Our Complete Community*.

PUBLIC FACILITIES AND INSTITUTIONS

The Public Facilities and Institutions area generally identified on **Map 9 – Land Use Concept**, includes schools, places of worship, recreational facilities, libraries, civic buildings, and other institutional

uses. Through the Area Structure Plan process, future school sites, parks, and other community services will be identified in the future residential areas.

PARKS, RECREATION, AND OPEN SPACE

Private Recreation

The Private Recreation area identified on **Map 9 – Land Use Concept**, identifies the Coloniale Golf Course and the Beaumont and District Agricultural Society operations. The Private Recreation policy area may also include any

privately operated recreation facilities that may be established within Beaumont's boundary in the future.

Parks and Open Space

The Parks and Open Space area generally identified on **Map 9 – Land Use Concept**, encompasses several existing natural and constructed uses that provide a public amenity, are public infrastructure, or provide protection of environmentally sensitive natural features. This includes public parks spaces and recreational

playing fields, wetlands, waterbodies, streams, and creeks, stormwater management facilities, utility corridors, and other public open spaces. Through the Area Structure Plan process, Parks and Open Space will be identified in the future residential areas.



4

Responsible Development





OVERVIEW:

Beaumont has grown at an unprecedented pace in recent years. To ensure that the community expands in an orderly, efficient, and responsible manner aligned with the Growth Directions of the Edmonton Metropolitan Region Growth Plan, Beaumont is committed to planned development and providing a diverse spectrum of land uses. For Beaumont to become a complete community we must recognize the importance of varied and integrated land uses, in addition to functional services that provide residents the ability to meet their everyday needs in a readily accessible way.

In February 2018, Beaumont entered into Intermunicipal Planning Framework Agreement with City of Edmonton and Leduc County. *Our Complete Community* recognizes this important collaborative planning efforts among the Three Partner Municipalities and will continue to participate in this initiative. As such policies around this Intermunicipal Planning Framework Agreement are included to guide further development in Beaumont.

KEY GOAL STATEMENT:

Growth and development will be undertaken in a manner that ensures Beaumont remains attractive, liveable, accessible, and safe, supporting inclusiveness, affordability and diversity for current and future residents, while encouraging a built form that promotes design excellence, innovation, and efficiency.

OBJECTIVES:

- a) Beaumont will participate in the Intermunicipal Planning Framework Agreement to collaboratively work with the City of Edmonton and Leduc County on future development with boundary interface and transportation and servicing efficiencies.
- b) Beaumont will provide a viable mix of residential, commercial, recreational and industrial development within the community.
- c) All development will utilize land and infrastructure wisely and efficiently.
- d) Beaumont will provide a choice of housing and tenure options, which meet the needs of a multi-generational, culturally diverse, and variable income community.
- e) Beaumont will encourage excellent urban design.
- f) Developments are resilient and designed to respond to climate change, economic change, and changing demographic conditions.
- g) Beaumont provides smart and flexible development regulation that supports development of complete communities.
- h) Beaumont will ensure that development complies with the greenfield density of 35 dwelling units per net residential hectare and strives to achieve the goal of intensifying the built-up residential area through a 10% increase in the number of dwellings, as per the Edmonton Metropolitan Region Growth Plan.
- i) Beaumont will continue its efforts to increase its non-residential assessment.
- j) Beaumont will plan to accommodate a population of 48,300 residents by 2044.

4.0 Responsible Development

4.1 General Policies

- 4.1.1 All development shall be in general accordance with the character areas identified on **Map 9 – Land Use Concept**.
- 4.1.2 All development shall be in accordance with an approved land use, infrastructure, and service plan.
- 4.1.3 Development shall integrate a broad spectrum of housing types to accommodate low, medium, and higher densities, which are described as the following:
- a) low density developments are intended to accommodate primarily single detached and semi-detached dwellings within residential neighborhoods;
 - b) medium density developments are intended to accommodate row housing and low rise buildings four stories or under with or without a mixed use component;
 - c) high density developments are intended to accommodate buildings greater than four stories with or without an integrated mixed-use component; and
 - d) all new developments in Future Residential areas shall provide a minimum of 30-40% of residential land use in the form of medium and high density development.
- 4.1.4 Compatibility of land uses proposed adjacent to oil and gas facilities, including sour gas facilities, wells, and pipelines shall be through the application of Provincial legislation, regulations, and guidelines. Beaumont will ensure appropriate development setbacks from oil and gas pipelines and facilities are achieved through the subdivision approval process.
- 4.1.5 Beaumont shall encourage cooperation with member municipalities and utility companies to create and maintain

integrated utility corridors.

- 4.1.6 Any proposed residential uses adjacent to highways and arterial roads shall require noise attenuation measures.
- 4.1.7 Cost of development shall be borne by the developers.

4.2 Statutory Plans

- 4.2.1 Area Structure Plans shall be prepared for the development of all Future Residential, Commercial, Business Park, and Light Industrial areas. Neighbourhood Structure Plans may be required to support Area Structure Plans for development areas larger than 64 hectares (one quarter section).
- 4.2.2 New Neighbourhood Structure Plans will not be permitted without an approved Area Structure Plan in place.
- 4.2.3 No new Outline Plans will be permitted.
- 4.2.4 The extension of urban development shall only occur if an approved Area Structure Plan is in place which demonstrates viable connections to services immediately adjacent to the lands proposed for development and development agreements are in place.
- 4.2.5 Redistricting of lands located in the Future Residential area will not be permitted without an approved Area Structure Plan in place. An approved Neighbourhood Structure Plan may also be required.
- 4.2.6 An Area Redevelopment Plan may be required prior to changes in built-up urban areas, at the discretion of the development authority.
- 4.2.7 Areas considered Built-up Urban Areas and Planned Areas, as defined on **Map 8 – Metropolitan Structure Components**, are grandfathered into *Our Complete Community* and will not be required to meet the greenfield area density targets

in the Edmonton Metropolitan Region Growth Plan. Substantive amendments to existing approved Outline Plans and Area Structure Plans will be required to meet the aspirational intensification target of 10% (exceptions may apply).

4.3 Regulatory Requirements

- 4.3.1 All proposed plans shall comply with the *Municipal Government Act* public consultation and notification requirement. Additional efforts beyond these requirements shall be encouraged where appropriate. This includes engagement of the public, the development industry and builders, adjacent landowners, and other stakeholders. Inclusive planning processes shall be required to provide understanding of development proposals and to provide the opportunity for review and feedback.
- 4.3.2 All new statutory plans or amendments to statutory plans shall comply with and indicate how they meet the requirements of the Edmonton Metropolitan Region Growth Plan and the Provincial Land Use Policies.
- 4.3.3 All new statutory plans or amendments to statutory plans shall be referred to the Capital Region Board in accordance with the requirements of the Regional Evaluation Framework process for approval.

4.4 Discourage Fragmented and Inefficient Development

- 4.4.1 Development shall maximize planned and existing municipal infrastructure and its ongoing operation and maintenance by utilizing existing capacity prior to consideration for expansion or extension of services.
- 4.4.2 Infill and redevelopment in existing serviced areas shall be prioritized, followed by areas where the extension of existing services and infrastructure is logical, contiguous, efficient, and economical.

4.4.3 Land will be intensified in the Built-up Urban Areas, as defined on **Map 8 – Metropolitan Structure Components**, through redevelopment, infill development, and expansion and conversion of existing buildings where opportunities exist. Where intensification of land in the Built-up Urban Areas is occurring, it shall be in compliance with the requirements of the Edmonton Metropolitan Region Growth Plan, including intensifying the built-up residential area through a 10% increase in the number of dwellings.

4.4.4 The premature fragmentation and development of agricultural land shall be prevented through the preparation of Area Structure Plans that provide for logical staging and phasing of development of the greenfield areas as shown on **Map 8 - Metropolitan Structure Components**.

4.4.5 In greenfield development areas identified for future urban development, only new uses that will not adversely impact future urban expansion and development shall be permitted. Existing activities will be allowed to remain until such time as the land is redeveloped for its intended use.

4.4.6 Within the Future Residential area, the fragmentation and conversion of prime agricultural lands for non-agricultural uses will only be considered when all of the following criteria are met:

- a) lands are contiguous with built up urban areas and/or planned areas;
- b) the development proposal is required to conform to the employment and population projections in accordance with the Edmonton Metropolitan Region Growth Plan;
- c) lands proposed for residential uses are identified within an Area Structure Plan that conforms with the minimum greenfield density of 35 dwelling units per net residential hectare;
- d) an Agricultural Impact Assessment has been completed as part of the Area

4.0 Responsible Development

- Structure Plan to identify potential adverse impacts of the proposed development on agricultural lands and existing agricultural operations both on-site and off-site in the surrounding area; and
- e) mitigation measures recommended through the Agricultural Impact Assessment conducted by a qualified professional are incorporated in the planning and design of the proposed development to minimize any identified potential adverse impacts.

4.5 Neighbourhood Design

- 4.5.1 Ensure that a diversity of housing types and tenure options, that meet a range of incomes and demographics, be incorporated into greenfield areas to meet the needs of all residents through all stages of life.
- 4.5.2 Mixed use developments that are vertically oriented and incorporate a mixture of housing, commercial, office uses, live/work units, and secondary suites will be encouraged.
- 4.5.3 Medium and higher density residential development should be located in areas with good access to major roads, trails and walkway connections, and within a five minute walk (400 metres) to transit opportunities.
- 4.5.4 New neighbourhoods shall be designed so that trails and walkways are integrated and connect to the community-wide pathway/trail network.
- 4.5.5 Natural features including tree stands, wetlands, streams and other natural features should be incorporated into neighbourhoods.
- 4.5.6 Gathering spaces within neighbourhoods shall be encouraged to provide safe places for children to play and foster community interaction and engagement

- 4.5.7 Road networks shall be designed to create walkable streets with frequent connections and linkages to destinations and other modes of transportation. The inclusion of cul-de-sacs shall be minimized.
- 4.5.8 Developments should be designed to incorporate winter design principles whenever possible. Winter design principles are meant to encourage the development of comfortable, accessible, and enjoyable public spaces in the winter season and throughout the year. They include:
- blocking wind;
 - maximizing sunshine exposure;
 - use of colour and light; and
 - providing infrastructure to encourage use and to provide comfort.

4.6 Safety and Accessibility

- 4.6.1 Crime Prevention Through Environmental Design principles should be considered for the design of new neighbourhoods, development and redevelopment of buildings, open spaces, pathways, and parking areas to enhance the effective and safe use of the space.
- 4.6.2 Developments should be planned and designed to be barrier-free and accessible to allow use by as wide a range of people as possible.
- 4.6.3 New development and redevelopment must demonstrate how emergency services will be accommodated.

4.7 Transit Oriented Development

- 4.7.1 Medium and higher density residential and mixed use developments shall be located near park and ride facilities or transit stops in the Future Residential area, Centre-Ville, and Community Commercial/Employment Centres to support high frequency, rapid transit service and provide a base for a variety of housing, employment, local

- services, and amenities that support a complete community and support ridership.
- 4.7.2 Convenient, comfortable, direct, and safe pedestrian linkages must be created at all transit stops and park and ride facilities. Implement innovative parking strategies to accommodate transit bus, private automobile, and cycling circulation, in addition to parking needs near transit stops and park and ride facilities.
- 4.8 Residential Policies**
Established Residential Area Policies
- 4.8.1 The planned and built-up residential areas of Beaumont are designated as Established Residential on **Map 9 – Land Use Concept**.
- 4.8.2 Neighbourhood re-investment shall be encouraged by taking full advantage of existing services and facilities in established residential areas by:
- encouraging the development of vacant lots and the redevelopment of underutilized lots;
 - striving to meet the aspirational intensification target of 10%;
 - encouraging secondary suites;
 - non-market affordable housing through collaboration with other member municipalities and non-profit housing providers on the funding and delivery of non-market affordable housing;
 - enabling re-subdivision of large (>15m wide) residential lots where appropriate; and
 - supporting incremental ways to improve our public realm.
- 4.8.3 Proposed development should be sensitive to the scale and design of the surrounding built environment to ensure suitable integration of new uses in existing areas.
- 4.8.4 The integration of home-based businesses and live-work uses in Established Residential Areas is encouraged. Applicants must demonstrate that adjacent land uses will not be negatively impacted.
- 4.8.5 Neighbourhood services such as day care facilities, municipal libraries, places of worship, community support services, and neighbourhood commercial developments such as convenience stores or smaller grocery stores should be considered in Established Residential areas.
- 4.8.6 Medium to high density residential, mixed-use residential and commercial developments in Established Residential Areas should be oriented to the street, along proposed transit-serving corridors and adjacent to pedestrian trail, and walkway networks.
- 4.8.7 Smaller forms of housing will be encouraged and integrated with single detached residential developments within built-up areas where sufficient municipal services (water and sanitary) capacity exists.
- 4.8.8 Redistricting, redevelopment, or subdivision proposals in Established Residential areas may require the development of an Area Redevelopment Plan and should have regard for:
- compatibility of height and massing with adjacent land uses;
 - non-market affordable housing through collaboration with other member municipalities and non-profit housing providers on the funding and delivery of non-market affordable housing;
 - connectivity and impact on municipal services;
 - provision of required parking; and
 - existing street layout and site planning.
- 4.8.9 In established residential areas, high density residential developments shall be combined with medium density residential developments to provide an appropriate visual transition between high and low density uses.
- 4.8.10 Isolated blocks of high density residential developments shall be discouraged.

4.0 Responsible Development

4.9 Future Residential Area Policies

- 4.9.1 Unplanned, greenfield areas designated for residential neighbourhood development are identified as Future Residential on **Map 9 – Land Use Concept**.
- 4.9.2 All Area Structure Plans within the Future Residential area must ensure that they conform to the minimum greenfield density established for Beaumont in the Edmonton Metropolitan Region Growth Plan of 35 dwelling units per net residential hectare.
- 4.9.3 No new development, or subdivision that may inhibit future urban development as shown on **Map 9 – Land Use Concept** shall be permitted.
- 4.9.4 New residential development in the Future Residential area must be planned to extend existing municipal infrastructure in a contiguous, efficient, and cost effective manner as established through Area Structure Plans.
- 4.9.5 The integration of home-based businesses and live-work uses in Future Residential areas may be considered. Applicants must demonstrate that adjacent land uses will not be negatively impacted.
- 4.9.6 New greenfield developments in the Future Residential area will strive to integrate residential development with local services, amenities, institutional, and commercial uses including mixed-use developments that are located within buildings or within a five-minute walk (400 metre radius) as established through Area Structure Plans.
- 4.9.7 Medium to high density residential, mixed-use residential, and commercial developments in the Future Residential area should be oriented to the street, along proposed transit-serving corridors and adjacent to the pedestrian trail and walkway network.
- 4.9.8 In Future Residential areas, higher density residential developments shall be combined with medium density residential developments to provide an appropriate visual transition between higher and low density uses.
- 4.9.9 Isolated blocks of higher density residential development shall be discouraged.
- 4.9.10 New residential areas within Beaumont shall be required to provide diverse housing options, services, and facilities that meet the needs of residents at all income levels and stages of life. This shall include the provision of a mix of single detached, semi-detached, row housing, multi-unit housing, and apartments integrated within neighbourhoods as well as allowances for secondary suites.
- 4.9.11 To provide affordable housing options and contribute to housing choice within the Future Residential area, Beaumont shall support the development of:
- more compact housing forms and increased density to achieve a more diverse housing stock in Beaumont;
 - development of secondary suites and innovative parking strategies;
 - incorporating innovative building practices related to prefabrication, building materials, and built forms with good urban design to improve affordability;
 - the formation of innovative partnerships and exploring creative financing models, regulations, standards and approval processes, and incentives for the private sector to provide market affordable housing options; and
 - non-market affordable housing through collaboration with other member municipalities of the Edmonton Metropolitan Region and non-profit housing providers on the funding and delivery of non-market affordable housing

4.10 Future Growth Area

4.10.1 Beaumont will actively participate in the Intermunicipal Planning Framework Agreement discussion with the City of Edmonton and Leduc County.

4.10.2 Future land uses around shared municipal boundaries of the City of Edmonton, Leduc County, and Beaumont to be included in the intermunicipal planning framework discussion to explore and identify recommendations for decision making around boundary interfaces, conceptual land uses, future transportation corridors and utility networks.

4.10.3 Development in the Future Growth Area will occur in a manner that implements the cost-shared servicing approach as identified in the Intermunicipal Planning Framework Agreement.

4.10.4 Efforts shall be made not to create hardships for the City of Edmonton and Leduc County to plan their respective lands within their municipal boundaries.

4.11 Centre-Ville

4.11.1 The Urban Centre or downtown core of Beaumont is designated as Centre-Ville, which is shown on **Map 9 - Land Use Concept**.

4.11.2 Centre-Ville will continue to be developed and revived with the goal of making Centre-Ville the major business, social, cultural, and entertainment focus of the community with a distinct French flair.

4.11.3 Centre-Ville will strive to maintain the heritage value and special character of streetscape, cultural landscapes, and individual properties through statutory and non-statutory instruments.

4.11.4 The Central Area Redevelopment Plan shall be reviewed and updated on a regular basis as defined in the Central Area Redevelopment Plan, to provide direction on:

- a) achieving a density target of 100 dwelling units per net residential hectare as outlined in the Edmonton Metropolitan Region Growth Plan;
- b) intensification;
- c) transit oriented development;
- d) high density mixed-use development;
- e) commercial services including business commercial;
- f) education services;
- g) local and major community services;
- h) multi-modal transportation including local and commuter transit services;
- i) preservation and enhancement of the active transportation networks;
- j) open space and pedestrian infrastructure;
- k) infrastructure; and
- l) parking.

4.11.5 Centre-Ville will accommodate a mix of employment and population growth through intensification and transit oriented development and will strive to achieve the Edmonton Metropolitan Region Growth Plan aspirational Urban Centre Density target of 100 dwelling units per net residential hectares in Centre-Ville over the horizon of *Our Complete Community*. The development of vacant lots and the redevelopment and intensification of underutilized lots in Centre-Ville to optimize existing and planned infrastructure shall be encouraged.

4.11.6 Centre-Ville will be pedestrian focused. New developments and redevelopments shall be designed using pedestrian-scale site planning and building design.

4.11.7 Parking shall be located under or behind buildings to preserve the continuity of the existing street face. Adjacent parking may be considered where other options are not viable.

4.11.8 Residential uses may be permitted above or behind ground floor pedestrian-oriented commercial/office uses, as well as medium and higher density developments.

4.0 Responsible Development

Commercial/office and mixed use development should include:

- a) separate at-grade entrances for commercial/office and residential premises when located in the same building;
- b) visually transparent façades along the ground floor;
- c) reduced front setbacks, with the exception of providing for patio and seating space; and
- d) building façades with design elements, materials, and variations that reduce perceived mass and linearity and add interest consistent with the Beaumont Urban Design Guidelines.

4.11.9 Residential development, including infill development, in Centre-Ville is encouraged to provide a diversity of housing options with a focus on increased density, type, form, and affordability including row housing.

4.11.10 Beaumont may allow for major (corporate) retail and restaurant uses within Centre-Ville that are developed in an urban format, fronting public streets, and with parking that is screened or located behind buildings. These uses must comply with the Beaumont Urban Design Guidelines regarding architectural design and signage.

4.11.11 Centre-Ville shall incorporate parks, open spaces and plazas, services, and facilities that are accessible, complement commercial uses, and act as destinations.

4.11.12 Existing single detached residential buildings, which could be converted to commercial or office uses, shall be permitted to remain, however the development of new single residential structures on those properties shall be prohibited within Centre-Ville.

4.11.13 Development within Centre-Ville will be encouraged to provide high quality design and aesthetics through:

- a) use of attractive and high quality

building materials, finishes, and façades consistent with the Beaumont Urban Design Guidelines;

- b) installation of enhanced landscaping;
- c) incorporation of public art;
- d) incorporation of Winter City design principles;
- e) public gathering spaces and parks; and
- f) transit and active transportation.

4.12 Community Commercial/Employment Centre Policies

4.12.1 The development of Community Commercial/Employment Centres in the locations indicated on **Map 9 - Land Use Concept** is actively encouraged. Typical land uses include a mix of retail, offices, and institutional uses and may also include multi-unit residential developments as standalone developments or integrated into the upper storeys of commercial or office buildings.

4.12.2 Streets and paths should be designed to accommodate a range of users including transit, active transportation (e.g. walking, cycling), passenger vehicles, and goods movement. Variances to Beaumont's standards may be considered to accommodate public realm improvements as set out in an Area Structure Plan.

4.12.3 Development in this policy area carries the expectation of higher quality site and building design. Area Structure Plans, Outline Plans, and development permit applications shall identify site design and architectural guidelines, which may

be further defined through architectural controls.

4.12.4 Public realm improvements including street furniture, public art and public gathering spaces, bicycle racks and storage facilities should be incorporated into developments within Community Commercial/Employment Centres.

4.13 Commercial Policies

- 4.13.1 The development of Commercial uses will be encouraged in the locations indicated on **Map 9 - Land Use Concept**. Uses include retail, office, hospitality, and entertainment uses, including large-format stores that attract a community-wide customer base along key transportation routes to provide access and exposure. Multi-unit residential developments may also be integrated into the upper storeys of commercial buildings.
- 4.13.2 Commercial uses shall be situated along major roadways (major collector and arterials) to ensure convenient access, freight movement, and to minimize land use conflicts.
- 4.13.3 Commercial subdivisions adjacent to highways shall be required to provide service roads and shall be in accordance with provincial transportation policies.
- 4.13.4 Commercial uses shall be oriented to align with public roadways to create a pedestrian friendly streetscape and engaging public realm.
- 4.13.5 Multi-storey commercial buildings that efficiently use land and infrastructure are encouraged.
- 4.13.6 New commercial development proposals shall be subject to:
- an approved Area Structure Plan;
 - An evaluation of servicing capacity and the provision of required infrastructure to be provided at the developer's expense;
 - traffic assessments for appropriate circulation and access; and
 - all costs of development shall be borne by the developer.

4.14 Business Park Policies

- 4.14.1 **Map 9 - Land Use Concept** identifies the Business Park areas, which establish the location for the development of

employment, office, commercial and light industrial uses (where all activities occur within the confines of the building and there is no outside storage). Hospitality uses like restaurants, convenience stores, and hotels may also be considered in the Business Park area.

- 4.14.2 New Business Park developments shall be encouraged to provide higher architectural and landscaping standards to establish a distinctive and cohesive image for the development. Beaumont may require a comprehensive design concept to address architectural controls, landscaping and open space, and parking design.
- 4.14.3 Buildings located in Business Park areas are encouraged to be vertically oriented and incorporate multiple stories of development for a variety of uses.
- 4.14.4 New Business Park development proposals shall be considered subject to:
- an approved Area Structure Plan;
 - An evaluation of servicing capacity and the provision of required infrastructure to be provided at the developer's expense;
 - traffic assessments for appropriate circulation and access; and
 - all costs of development shall be borne by the developer.

4.15 Light Industrial Policies

- 4.15.1 **Map 9 - Land Use Concept** identifies the Light Industrial areas, which establish the location for the development of Light Industrial uses.
- 4.15.2 To prevent or mitigate potential impacts to adjacent non-industrial uses on adjacent sites, ensure sufficient buffering / separation space is provided through provisions in the Land Use Bylaw.
- 4.15.3 A risk assessment will be required, which may include a hazardous materials

4.0 Responsible Development

impact analysis and/or an environmental assessment, at the cost of the developer, when considering an industrial use that may pose a potential safety hazard or environmental nuisance at Beaumont's discretion.

- 4.15.4 Light Industrial areas shall have direct access to truck routes and highways. Commercial traffic should be routed to avoid residential areas.
- 4.15.5 New Light Industrial development proposals shall be subject to:
- an approved Area Structure Plan;
 - an evaluation of servicing capacity and the provision of required infrastructure to be provided at the developer's expense;
 - traffic assessments for appropriate circulation and access; and
 - all costs of development shall be borne by the developer.

4.16 Municipal Servicing and Utilities

- 4.16.1 The maintenance and completion of servicing infrastructure upgrades and construction will be in conformance with approved servicing studies, plans, and standards to support high quality service provision and future development capacity.
- 4.16.2 Servicing shall be extended to new development in a contiguous, logical, and efficient manner.
- 4.16.3 All new development shall connect to municipal services where appropriate.
- 4.16.4 Developers shall be required to construct and pay for all or a portion of the service connection and oversizing costs associated with new development. Off-site levies and cost sharing agreements will be established to ensure proportionate contributions from benefiting lands.
- 4.16.5 Sustainable and innovative servicing

practices, technologies, and design standards to increase efficiency and effectiveness while reducing environmental impacts will be encouraged.

- 4.16.6 In addition to this plan, all new development and redevelopment shall be guided by Beaumont's Utility Master Plan.

4.17 Private Recreation

- 4.17.1 The Private Recreation area, located at the SW ¼ Section of 33-50-24-W4M, shall remain agricultural in use until such time that the landowner proposes a different use. An approved Area Structure Plan shall support any change of use or redistricting on these lands. An approved Neighbourhood Structure Plan may also be required.
- 4.17.2 Applications for the development or redevelopment of private recreation operations may be considered, having regard for existing and planned infrastructure, compatibility with adjacent land uses, and adequate site access. Redevelopment of Private Recreation shall be in conformance with an approved Area Redevelopment Plan. An approved Neighbourhood Structure Plan may also be required.

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5

Economic Strength





OVERVIEW:

Residential development has driven Beaumont’s growth for many years. While residential expansion will continue, diversification of Beaumont’s local economy is essential to ensure that future growth and development is fiscally sustainable while contributing to the diversification of the Edmonton Metropolitan Region. The cost for additional infrastructure, services, and community programs can no longer be placed on residential development only and a greater percentage of growth must be achieved within the commercial, business park, and light industrial sectors. Beaumont’s close proximity to the Edmonton International Airport, Nisku, as well as corporate headquarters and centres for higher education in Edmonton provides the opportunity to leverage complementary business development. Beaumont’s unique positioning also gives it the opportunity to promote job creation in the information and communication technologies, clean-tech, healthcare, agri-innovation, science/research, professional services, and tourism sectors. The provision of a wide range of local employment opportunities, diversification of the tax base, and access to a range of goods and services helps contribute to transforming Beaumont into a complete community that is dynamic and sustainable.

KEY GOAL STATEMENT:

A healthy economic base sustains a vibrant community for residents and local business.

OBJECTIVES:

- a) Diversify the economic tax base by attracting diverse and unique commercial / industrial / business uses.
- b) Foster local employment opportunities that enable residents of all capabilities to live and work in the community.
- c) Increase economic strength by striving to:
 - I) Achieve an 80/20 tax base;
 - II) Increase the diversity of commercial uses such as national chains, hotels, and shopping opportunities;
 - III) Foster local employment opportunities and support the strong local business/entrepreneurial spirit in the community;
 - IV) Promote the growth of sectors related to health, the knowledge-based economy, and professional service in employment areas to contribute to global economic competitiveness and diversification of the regional economy; and
- d) Promote Beaumont as a tourist destination acknowledging the rich Indigenous, French, and agricultural heritage, encouraging celebrations, festivals, cultural activities, etc.
- e) Encourage an environment of innovation, entrepreneurship, skill development, and life-long learning.
- f) Plan to accommodate 8,022 jobs by 2044 as projected in the Edmonton Metropolitan Region Growth Plan.
- V) Promote “live and work in Beaumont”.

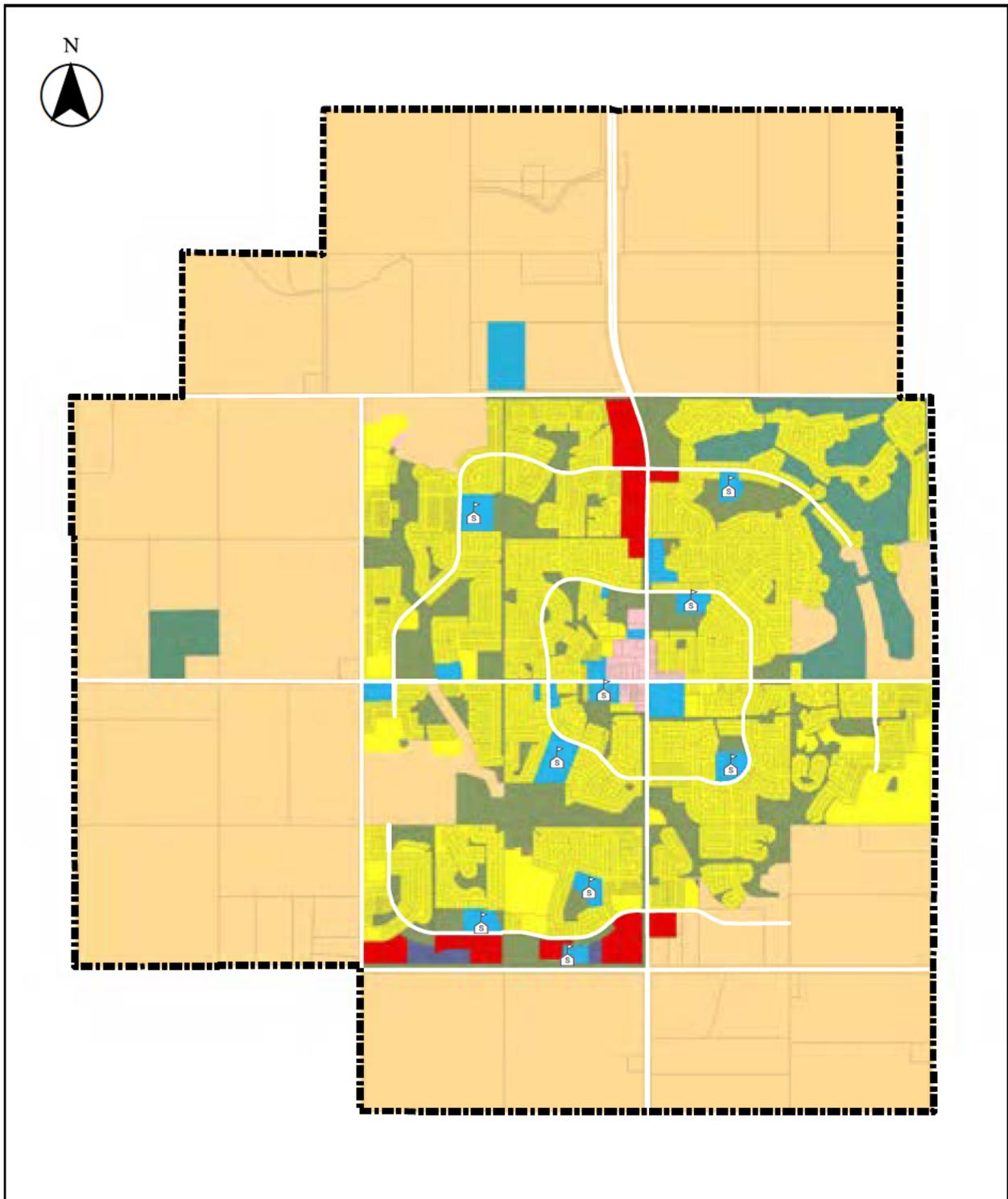
5.0 Economic Strength

5.1 General Policies

- 5.1.1 Areas shown as Commercial, Business Park, and Light Industrial on **Map 9 – Land Use Concept** are considered Local Employment Areas as per the Edmonton Metropolitan Region Growth Plan as shown on **Map 8 – Metropolitan Structure Components**.
- 5.1.2 Provide opportunities for economic growth with a focus on light industrial, business offices, service and supply operations, value-added agriculture (agri-innovation), eco-industrial services, health campuses, knowledge-based economy companies, engineering/professional services (knowledge based industry), and technology development within the Light Industrial, Business Park, and Commercial areas near major transportation corridors to enhance regional and global connectivity, take advantage of existing infrastructure, and make more efficient use of the land base through intensification.
- 5.1.3 Encourage economic growth and diversification on the lands depicted on **Map 8 – Metropolitan Structure Components** to promote job growth and the competitiveness of the region's employment base.
- 5.1.4 Strive to accommodate 8,022 jobs within local employment areas by 2044.
- 5.1.5 Business Park and Light Industrial developments will be promoted south of Highway 625 and designed to take advantage of the high load corridor. Access management will be considered as part of the Area Structure Plan to protect the integrity and function of the high load corridor.
- 5.1.6 The development of storefront commercial businesses, including: diverse shopping opportunities, professional services, personal services, national chains, niche market businesses, and neighbourhood-based businesses will be encouraged.
- 5.1.7 Foster a welcoming environment for regional businesses that do business in the community and maintain a local business license, recognizing that over time, some of these businesses may expand or wish to relocate to Beaumont.
- 5.1.8 Local businesses that access a significant niche market and international opportunities will be encouraged.
- 5.1.9 Engage with Indigenous peoples on projects or initiatives that intersect with identified Indigenous interests and concerns.
- 5.1.10 Participate in regional economic development bodies such as Edmonton Global, the Leduc-Nisku Economic Development Association and the Greater Edmonton Economic Development Team and work proactively with member municipalities and regional stakeholders to pursue economic development opportunities and positively promote the Edmonton Metropolitan Region and Beaumont's unique position within it.
- 5.1.11 Support local artists, artisans, crafters, and other individuals who develop works that are reflective of Beaumont's heritage, and encourage them to locate in Centre-Ville and other mixed-use and live-work districts such as Community Commercial/Employment Centres.
- 5.1.12 Home Based Businesses will be encouraged, in recognition that many local entrepreneurs start as home based operations and that a percentage of these businesses, may grow into storefront businesses.
- 5.1.13 Continue to develop and promote Centre-Ville as a destination, with a focus on destination retail, hospitality, accommodation, and food service industries.

- 5.1.14 Foster tourism and support the local economy by promoting festivals, cultural activities, and events that are inclusive and respectful of Beaumont's heritage.
- 5.1.15 Work with the local business community to increase the capture and retention rate of local disposable income, increase the level of retail expenditure inflow, and reduce the level of retail expenditure outflow.
- 5.1.16 Local employment opportunities which will enable residents to live and work, in the community will be supported.
- 5.1.17 Support the local business community by working closely with the local and regional business groups to understand the needs, desires, and opportunities for local businesses.
- 5.1.18 Neighbourhood commercial uses will be encouraged to locate in Future Residential Policy areas in centralized locations adjacent to multi-modal transportation corridors.
- 5.1.19 The conversion of local employment areas to non-employment uses with the exception of Centre-Ville shall be evaluated as part of a comprehensive review through an amendment to *Our Complete Community*. Proposals for conversion shall demonstrate that:
- a) the lands are not required over the horizon of *Our Complete Community* for employment purposes;
 - b) there is sufficient employment lands to accommodate projected employment over the horizon of *Our Complete Community*;
 - c) the conversion and proposed uses will not adversely impact the overall viability of the local employment area; and
 - d) there is existing or planned infrastructure to accommodate the proposed land use.

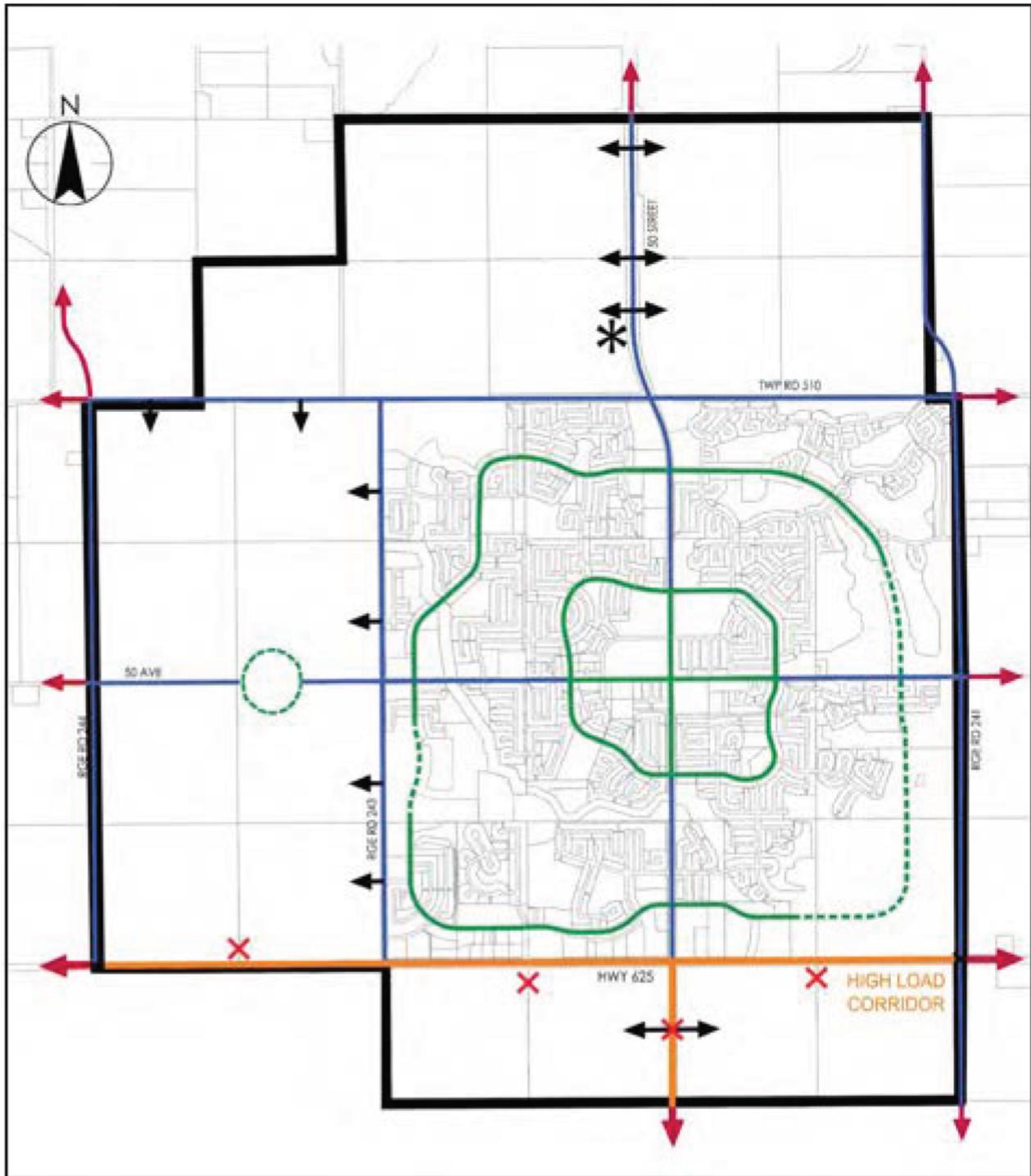
Map 3 Current Land Use



Legend

- | | | |
|---|--|---|
|  School |  Business Park |  Institutional and Public Facilities |
|  Residential |  Public Recreation |  Municipal Boundary |
|  Commercial |  Private Recreation | |
|  Mixed Use |  Agricultural Reserve | |

Map 7 Transportation Network



Legend

- Collector (Major)
 - - - Future Collector (Major)
 - Arterial
 - Highway
 - Intersection / Crossing Locations
 - X Potential Highway Access
 - Future Connections
 - * Possible Location for Regional Park & Ride
 - Municipal Boundary
- * Conceptual Access Strategy. Access Management will be further defined in the Transportation Master Plan.*

What We Heard & Did

Report for Proposed Development at 9 Coloniale Way

December 29, 2025

Introduction

The City of Beaumont received a development permit application and a redistricting application for 9 Coloniale Way in mid-2025. These are two separate but related processes:

1. The development permit authorizes the construction of two commercial buildings with tenant spaces for a range of potential businesses, including a daycare.
2. The redistricting application proposes changing the site's land use designation from Integrated Neighbourhood (IN) to Commercial (C).

Landowners have the right to apply for development in accordance with the Land Use Bylaw, Municipal Development Plan, and other plans adopted by Council. Administration is obligated to evaluate such applications against the City's plans, policies, and regulations when making its decision.

Both districts allow commercial development on this site, including a daycare as a discretionary use. The proposed buildings could be approved under either district and the development permit review considers the same criteria whether the land is designated IN or C. The key difference is that the C District allows a broader range of commercial uses in the long term.

The parcel at 9 Coloniale Way has been identified for commercial development since the inception of the Coloniale Estates neighbourhood in the 1990s. While most of the surrounding neighbourhood has built out, this site remained largely undeveloped. Earlier this year, the property was sold to new owners who submitted the current development permit application.

Once the required information was provided to support the application, it was deemed complete on July 22, 2025. Administration reviewed the application against the Land Use Bylaw, the Beaumont Urban Design Guidelines, and the General Design Standards and found that the proposal aligns with the applicable requirements in each. Administration requested revisions to improve the entrance orientation and façade treatment along 50 Street and Coloniale Way, and the applicant provided updated drawings to reflect those changes.

In this case, the development includes a daycare, which is a discretionary use in both the IN and C Districts. For discretionary uses, Administration must determine whether the use is reasonably compatible with surrounding development.

If the proposal does not meet a land use bylaw requirement, Administration may refuse the application or grant a variance to that requirement. Administration must consider whether that variance would result in an undue impact on neighbourhood amenities or a materially adverse effect on the use, enjoyment, or value of nearby properties.

Process and Timelines

Two separate processes are happening at the same time:

- Development permit – This decision is made by Administration and authorizes the development and enables the developer to construct the building subject to any conditions of the permit and after obtaining a building permit.
- Redistricting (also referred to as Rezoning) – This requires a decision by Council and determines the types of uses that can take place on the land.

Both applications were received at the same time and are being processed together, but the outcome of one does not affect the other. Because a commercial use is permitted in an IN District, the application to change the land to a C District will not determine whether the proposed project can be built. Even if Council *does not* approve the application to redistrict the land, the development permit could still proceed.

Development Permit Process and Timeline

What it is:

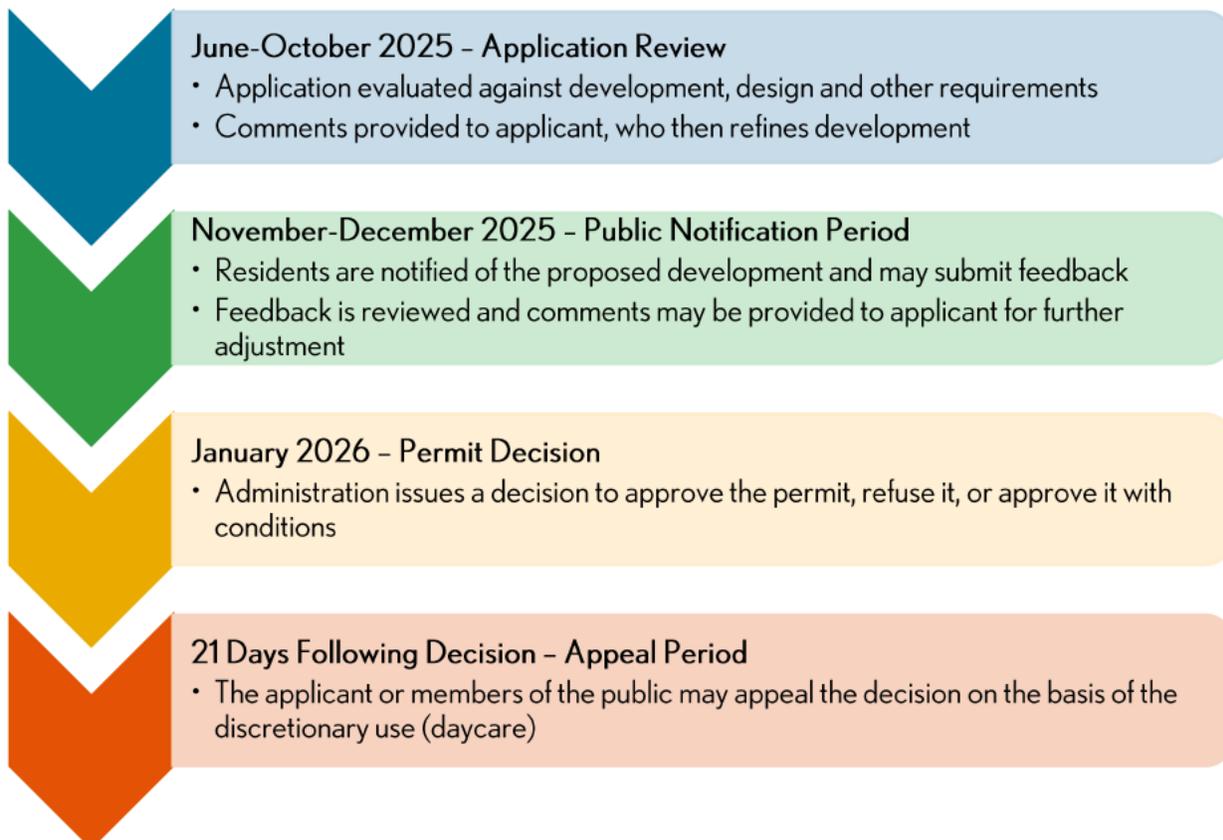
- The application that asks, “*Can the buildings be constructed as shown on the plans and are the proposed uses allowed?*”
- It is reviewed by Administration (the Development Authority).

What it decides:

- The buildings’ size, height, layout, parking, entrances, landscaping, and design.
- Whether the proposed daycare (a discretionary use) is reasonably compatible with the surrounding area.
- Whether variances, if any, do not unduly interfere with or affect the use, enjoyment, or value of nearby properties.

Important:

- A development permit can be approved under either IN or C zoning.
- The proposed buildings meet the requirements to be considered under both districts.
- The development permit is not decided by Council.
- If approved, residents can appeal to the Subdivision and Development Appeal Board.



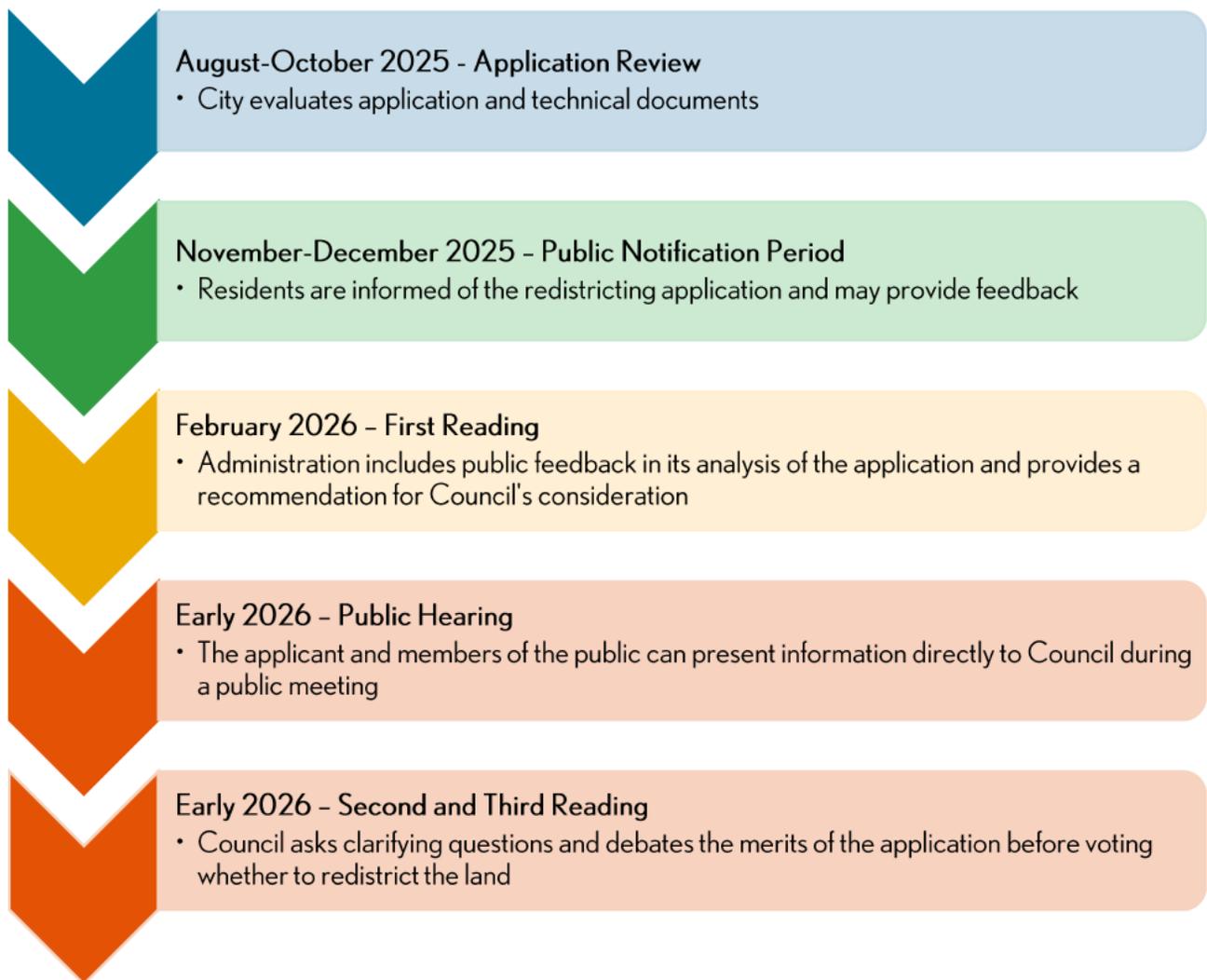
Redistricting Process and Timeline

What it is:

- A request to change the land's zoning from Integrated Neighbourhood (IN) to Commercial (C).
- This must go to Council for First Reading, Public Hearing, Second, and Third Reading.

What it decides:

- The range of future commercial uses allowed on the site.
- The C District allows more types of commercial uses, but does not change the ability to build the two buildings proposed.
- While the District allows for different uses, the redistricting does not authorize those uses in the development. That would need to be done through a separate development permit application.



Pre-Decision Engagement

As part of the City’s review process, a sign was posted on the site to inform the public of the proposed development and invite public feedback. The intent of this process is to increase transparency and provide an opportunity for nearby residents to offer comments before a decision is made. In this case, the sign was placed on November 7, 2025 and remained for 28 days until December 5.

The City received 51 unique responses from the public. The approximate overall sentiment, based on the primary position stated, was:

- Opposed: ~39-43
- Supportive: ~1-2

Another 6 to 8 submissions were supportive of some form of development, but expressed concerns about the scale of the development, access to the site, traffic, mix of uses, or sought changes, such as fewer units, single-storey massing, traffic controls, or adjustments to the design and landscaping.

The volume and tone of responses indicate a high level of neighbourhood interest and concern. Collectively, this feedback helped Administration understand how residents experience the site today, what they value about the character of Coloniale Estates, and which issues they see as most important to consider in Administration’s review.

Engagement Theme Summary

Administration reviewed all feedback received through the pre-decision engagement process. Many of the concerns raised by residents were already examined as part of the technical review. The table below summarizes the major themes identified in community feedback and outlines the steps the City has taken to assess, address, or incorporate these concerns within the development permit review.

| Theme | What We Heard | What We Did |
|-------------------------|--|--|
| Traffic and road safety | Strong concern that the development would worsen congestion and turning movements on Coloniale Way and Rue Bouchard, particularly during school times, sports events, and peak commuting. Many referenced safety risks for children walking or cycling, and difficulty at the Coloniale / Rue Bouchard / Reichert intersection. Some requested consideration of mitigation, such as additional traffic control, intersection changes, turn restrictions, or traffic calming on Rue Bouchard. | The City required a Traffic Impact Assessment (TIA) to evaluate traffic generated by the development. The TIA concluded the surrounding roads can support anticipated traffic volumes. The purpose of a TIA is to identify necessary off-site improvements, not to determine whether development rights exist. Public concerns about access and egress are being evaluated against the TIA findings. The City also required a swept-path analysis (a modelling tool showing that emergency vehicles, such as fire trucks, can maneuver). |

| | | |
|--|--|---|
| | | <p>The feedback received regarding concerns with existing traffic and pedestrian safety in this area have been sent to Municipal Enforcement and Infrastructure for further review.</p> |
| <p>Development scale, intensity, and orientation</p> | <p>The proposal is widely seen as “too much on too small a site” (e.g., references to “19 units”, two storeys, and “mass size”). Many requested a smaller project, fewer units, or single-storey development more in keeping with the area. Some requested that the building orientation face Coloniale Way or 50 Street.</p> | <p>The application meets or exceeds all requirements related to setbacks, site coverage, landscaping, and parking. The orientation supports pedestrian activity on Rue Bouchard. The City worked with the applicant to improve the Coloniale Way frontage with upgraded materials, increased glazing, and some north-facing units to enhance the gateway appearance.</p> |
| <p>Alternative uses for the site</p> | <p>Numerous respondents suggested different uses: community gardens, green space/park, additional parking to support field use, seniors’ housing, small-scale residential, boutique hotel, indoor play space or bowling, or a modest commercial/office building with far fewer units. Some explicitly opposed changing the land use to Commercial.</p> | <p>The City does not own this parcel and cannot determine the application type. The land has been identified for commercial development since the original Coloniale Estates Plan. As with any private property, the landowner has the right to apply for development consistent with the Land Use Bylaw and other plans adopted by Council. This review process ensures the proposal is evaluated against all applicable requirements.</p> |
| <p>Neighbourhood character and compatibility</p> | <p>Concerns that a two-building commercial plaza at the gateway to Coloniale Estates would undermine the feel and character of the existing neighbourhood and entrance. Several stated that it does not align with their perceived original vision for the subdivision or with what residents understood when they purchased their homes.</p> | <p>The application was reviewed against the Beaumont Urban Design Guidelines and by the Design Review Committee. The City worked with the applicant to upgrade materials, articulation, and glazing. Significant improvements were made to the Coloniale Way frontage, including enhanced façade treatments and additional glazing.</p> |

| | | |
|---|---|--|
| <p>Parking adequacy and spillover</p> | <p>Many felt on-site parking would be insufficient for staff and customers, leading to spillover parking on surrounding residential streets and near the park/soccer fields, compounding existing game-day congestion.</p> | <p>The City evaluated the proposal using the highest possible parking requirements under the Land Use Bylaw. The development requires 50 parking stalls and provides 82, exceeding the required parking by 32 stalls (61 per cent over minimum).</p> |
| <p>Business types / allowable uses</p> | <p>Requests to avoid or limit certain uses (vape, cannabis, and liquor stores; nail salons; pizza or other fast food uses). Some asked for more unique, wellness-oriented, or family-oriented businesses if commercial proceeds at this location.</p> | <p>The City received interest from various business types during circulation. Economic Development continues to explore viable options with the landowner. Landowners have the right to lease to any business permitted in the Land Use Bylaw. Market conditions ultimately determine tenant mix.</p> <p>Allowable uses are determined through the zoning, Administration will bring this feedback to Council to be considered as part of the redistricting application from IN to C District.</p> |
| <p>Property value, privacy, noise and lighting</p> | <p>Concerns about two-storey commercial buildings backing onto homes, potential loss of privacy and sunlight, additional noise (especially evenings), lighting shining into yards/windows, and possible negative effects on property values.</p> | <p>Property values are influenced by many market factors beyond adjacent land use. The City reviewed the design to mitigate impacts through lighting controls, screening, and site layout requirements.</p> <p>The City also required a lighting plan meeting Dark Sky Association guidelines to minimize spillover into neighbouring properties.</p> |
| <p>Daycare suitability (as a discretionary use)</p> | <p>Questions and concerns about whether the site design properly accommodates a daycare (fenced outdoor play space, appropriate proximity to the park, safe pickup/drop-off circulation, and regulatory viability).</p> | <p>Daycares are provincially regulated. The proposal shows over 4,500 square feet of outdoor play space on the second floor. Provincial regulators, not the City, determine compliance. The City reviewed site circulation and design to ensure compatibility with the development permit requirements.</p> |

| | | |
|-------------------------|---|--|
| Adequate infrastructure | Concerns about whether the City's existing utility infrastructure could support the development and who would pay for it. | The City required a servicing memo for water and sanitary capacity. The memo confirmed that current servicing has adequate capacity to support the proposed development. |
|-------------------------|---|--|

Conclusion and What This Means Moving Forward

The pre-decision notification process generated significant community feedback. This is the purpose of the notification requirement: to create transparency around proposed developments and provide residents with an opportunity to be heard before a decision is made.

All the feedback, whether focused on traffic, neighbourhood character, parking, safety, building interface, or overall fit, helps Administration understand the potential impacts of the proposal from the perspective of people who live in the community. This input plays an important role in informing Administration's review.

Under the Municipal Government Act and the Land Use Bylaw, Administration must evaluate the application through two primary considerations:

- whether the proposed discretionary use (daycare) is reasonably compatible with surrounding development; and
- whether any elements of the proposal that may require a variance would result in an undue impact on neighbourhood amenities or a materially adverse effect on the use, enjoyment, or value of neighbouring properties.

This pre-decision engagement process reflects the City's commitment to transparency and early communication. It allows residents to be informed and heard at the outset, rather than only after a decision has been made, and ensures that community perspectives help shape the evaluation of the development proposal.

Based on the review and feedback, Administration expects to finalize its review and issue a decision in early January. Notification of the decision will be posted publicly to the City of Beaumont website.

Appendix – Public Submissions

Good day,

I am writing you regarding the proposed commercial development. I am a resident [redacted].

I am opposed to the development and proposed land use change. There are a number of reasons for my decisions.

Traffic

This is a major entry point for residents and students. The area already suffers from congestion during commuting hours, then school, followed by outdoor activities in the sports field/park across the street, and commuting again. This would substantially increase the traffic to an already crowded 2 lane street. In order to avoid this congestion people will resort to using 55 Ave, passing the daycare and school. This will reduce the safety in this area where there are young children entering and exiting the daycare and school, as well as school bus loading. Getting people to obey the school zone speed limits is already difficult in this area. It would only get worse with increased traffic.

Unnecessary additional commercial properties in a small community that is saturated with repeat category businesses. As well, we still have a number of vacant, poorly planned commercial units that need tenants before we build more. There is a zoned open area next to the Wendy's that is more suitable for additional stores if we really want to saturate our city.

The citizens of Beaumont have made it very clear the reason we are living in this community is due to its small town feel. We are proud of the community we live in, yet we don't show it. We have allowed the greed and manipulation of previous sitting Government's and Developers to come in and over crowd our city. We have eliminated green spaces, parks, and natural habitats for the animals that have been here for years. Aside from a dog park, we have not created any green spaces to enjoy. What would be wrong with creating a gateway to our community with a maintained park with benches, trees, flowers and paths. In the busiest of cities all through Europe they have community parks where people gather. To be clear, I don't consider the development with the fireplace a proper park setting. It is bare and has absolutely no beauty to it at all. The park could have a well thought out entry sign signifying our beautiful city and show people what a special community we have.

If it is allowed to be developed into commercial units the look won't be much different than the way it looks now - an eyesore. Barren, half completed projects, garbage, owned by derelict owners.

We have an opportunity to steer this city back to a community that we love and are proud to live in. We know what we have here in our community, please don't ruin it any further.

Kind regards

I have deep concerns about the project Reference # 2025-077 in Coloniale Estates on the corner of 50th street and Coloniale Way, Beaumont.

1. **Traffic safety:** There is always a lot of traffic, cars, school buses, and children, when people leave the suburb at rush hour, and return to the suburb at night, also during drop off and pick up at the elementary school just down the road, and added to whenever there is soccer game on the field, plus children playing on the small playground also on the field. The city has already caused students to cross three roads instead of one because of where they placed the latest cross walk on Coloniale Way. If anyone from the city would come and observe the intersection of Coloniale Way, Rue Bouchard and Reichert Drive (three big roads), they will see kids coming from school having the good sense to ignore the new cross walk and cross where the original cross walk was as it means the kids only have one road to cross, not three. Also, by moving the cross walk where it now is, on the west side of the intersection, children have to pay attention to cars coming from the busiest side of Coloniale Way to turn into Reichert Drive or Rue Bouchard, while, where the original cross walk is, the kids don't have to deal with the cars that turn into these busy roads.

To add a shopping center into this **poor and narrow traffic flow area** will cause a major problem for those trying to exit or enter the suburb, and make it dangerous considering the school, the sports field and the playground all in the same crowded area. Were the retail owners expecting cars to enter Colonial Way to access their stores? That is just too much traffic flow for this area. **It is unsafe and dangerous.** On 50th street the flow is one way with no way to cross 50th street at this point. I see even a day care is listed as a building- imagine more drop off cars and small kids in this crowded area!

One of the big problems causing traffic back up on Coloniale Way right in this **exact area** where the retail is hoping to build, is when people try to exit the suburb. The current road signs (from left to right) give drivers the following options: left only, straight ahead only, straight ahead or right turn. What this means is that drivers going straight can block the far-right turn lane. This blockage then causes traffic to pile up behind the driver determined to go straight from the far-right lane. This then causes a knock-on effect of stopping any traffic coming out of Reichert Drive, and all traffic coming out of Rue Bouchard, and parents leaving the school. This **congestion** then causes a crowd of cars and school buses exactly where small children are crossing the road at the school and before. Changing the far-right lane to a far-right turn only would help with this problem. It is a narrow road. Coloniale Way narrows quickly back from 50th street.

I would need to be convinced the traffic into and out of this retail centre, given all the issues with this intersection, have been solved before I would even consider retail for this area.

Bad Impression: To start, this corner is the first impression anyone coming into Beaumont from Edmonton (travelling south) gets of Beaumont. Here they see what Beaumont values.

What they see is retail fully occupying the right-hand side; gas bars, and fast-food outlets...

On the left side, at the moment they see lovely sports field and a beautiful wall that says Coloniale Estates and Golf Course community. This is the only break in the ribbon retail development that is 50th street Beaumont all on the right side until we get to downtown, when the retail is now on both sides of 50th.

I don't think Beaumont needs to allow retail to fully take over this intersection in our city. It seems Beaumont is about retail and not homes. And Beaumont doesn't care about first French impressions.

Just as a note - For this space, I dreamed of a set of French looking row homes, with back parking. **The entrance to the homes would be off Rue Bouchard.** The reason for this is the **traffic congestion** that already occurs at the entrance to Coloniale Estates. If, after all the problems with traffic, the retailers get to go ahead, why are they not required to make the place look French?

I do not think enough thought has gone into this location. It is a special location and congested. It is not the place for retail.

Regards

[redacted]

Good Day [redacted],

I am writing to express my strong support for the City of Beaumont's initiative to change the land use designation at 9 Coloniale Way in Coloniale Estates from its current residential integration into a commercial district. This forward-looking decision will create opportunities for vibrant community amenities such as restaurants, cafés, daycare facilities, shops and wellness spaces, all of which align with Beaumont's vision of fostering a dynamic and inclusive community.

As a local Beaumont resident, I see firsthand the value this change will bring to our city. It will not only enhance convenience and lifestyle options for residents but also strengthen Beaumont's identity as a progressive and community-focused place to live.

Within this exciting development, I would like to formally propose my interest in investing in and establishing a luxury Pilates and hot yoga studio. Over the past several months, I have been conducting research and planning to bring this concept to life, and I believe Beaumont represents the ideal target market for such a studio, with this being the perfect opportunity to start the conversation.

- **Community Value:** Residents currently travel outside Beaumont for premium wellness services. A local studio would meet this demand while fostering stronger community connections.
- **Demographic Fit:** Beaumont's growing population of families, professionals, and health-conscious individuals makes this the perfect environment for a studio focused on health and wellbeing.
- **Complementary Offering:** A luxury Pilates and hot yoga studio would seamlessly complement the proposed restaurants, cafés, shops, and daycare facilities, creating a holistic hub where residents can dine, shop, care for their families, and invest in their personal health—all within walking distance.
- **Economic Value:** Beyond serving local demand, the studio would attract visitors from nearby communities, contributing to Beaumont's economic vitality and reinforcing its reputation as a family and wellness-oriented city.

I am deeply committed to this project and would be honored to serve as the investor and operator of the studio. I am confident that this studio will not only meet the needs of Beaumont residents but also enhance the city's appeal as a vibrant, health-conscious, family-oriented community.

I understand that this project is still in the early stages of development and that you may not be the final decision-maker for proposals of this nature. That said, I would love the opportunity to explore and begin the conversations of bringing this vision to life. I kindly ask to be considered as an investor and operator of a value-added service for the community once commercial development moves forward. I have attached a copy of my resume in case you'd like to have some background knowledge on me. Please do not hesitate to reach out if you have any questions or concerns.

Warmest Regards,

[redacted]

Hi,

Please note, I am completely opposed to rezoning the land in Coloniale for a commercial development. There are many families with small children as well as a soccer field right across the street, and a school just down the road. With these two sites already in place, there is already a high level of congestion in this area. Please do not rezone. This land should be developed in line with it's current zoning with something that will enhance the neighborhood, not cause more congestion.

Thanks,

[redacted]

Concerns:

TRAFFIC/NOISE/AESTHETICS/ PROPERTY VALUE

Colonial Way/Bouchard are high traffic areas due to school hours and adding to it will be this development's daycare drop off and pick up hours. Will there be enforceable no parking signs on Bouchard and/or Colonial (especially Colonial)? When people have Garage Sales they park on Colonial. Parents park on Bouchard to meet kids coming through the pathway at my residence and it is very difficult to get out of my driveway during peak hours. Also during summer months parents park on Bouchard/Bonin for sporting events, many times blocking or too close to the crosswalk. Visibility at the stop signs is very often hindered.

The development proposal also indicates Restaurant(s) which also means more dumpster of food waste etc. and a lot more traffic. If there is a restaurant tenant there will be more traffic, noise and not just during business hours but later into the night. A lot more parking on Colonial or Bouchard. The corner of Colonial and Bouchard is dangerous making the left turn onto Colonial during peak hours due to heavy school traffic and would be more so in the case of restaurant patrons. This unit has 81 parking stalls for shopping and office? 81 assigned parking stalls are not going to be enough especially if a restaurant is one of the tenants.

3 entrances Bouchard/Coloniale Way and 50 Street. Will there be a right turn only when exiting onto Coloniale Way? In which case U-turns will become regular. I view an entrance/exit onto Colonial which is not far down from the lights (50th/Colonial Way). Once again left hand turns will be extremely dangerous.

The backs of the building will be facing Colonial Way - dumpsters for a two-story building located at the rear or side not to mention loading and unloading. Unsightly and driving down property value.

Daycare play area facing Bouchard - Noise/drop off/pickup traffic.

What was the original zoning for that lot? When did it get changed from Integrated Neighborhood to Commercial Use?

Who is the developer for this site?

How long will construction take with extra heavy duty trucks, etc? Will traffic into Colonial Estates be rerouted?

Why is the strip mall two story and not one story like the strip mall units across 50th street or like Le Reve development which is located far from the roadway and away from the residences which are farther back. Le Reve has a smaller amount of units but it is on bigger parcel of land. This development is right up against houses on Bonin Crescent. I understand Center on 50th street because that is a commercial area only but even that is extremely congested even though there is ample parking at the rear. 50th Street across from Colonial Estates has all single story units, very conducive to high traffic area with more than ample parking. There is really no need to put a strip mall across the street from 50th Street gas stations and stores including large anchor stores. Another good thing about those strip mall units is that parking for tenant owners/ workers etc are locating is at the back of the units.

This building development is too large for a residential area. This unit has 81 parking stalls for shopping and office?

Will there be additional heavy duty lighting in the parking lot shining directly into backyards or front windows across the street?

Currently there is permanent signage (Coloniale Estates) at corner of 50th Street and Colonial Way, will that signage remain or be removed? If removed what will it be replaced with?

Thank you for consideration to this matter. Personally I think you should sell the lots to private citizens for home building to complete the beautiful area across from the park. Rezone to residential or smaller single story strip

mall. Don't know if you could squeeze as many houses as you are squeezing stores into this area but that would be my preference as we have more than enough mom and pop shops in this area AND we are right across the way from enough shops/restaurants that already caters to high traffic and has ample parking.

[redacted]

My name is [redacted]. I'm writing to voice my concern regarding the application for a development change and land use at 9 Coloniale Way.

As a resident of that street, I have seen how busy that part of Coloniale Way can be with north and southbound traffic coming off 50th St and have seen a lot of congestion at the corner of Coloniale Way and Rue Bouchard as that road is a feeder to other areas of our community. Having a commercial lot at 9 Coloniale Way would provide additional congestion. This is then compounded with foot traffic from Spring to Fall with the use of the football/soccer field on the north side. I would suspect that after enough complaints about traffic, traffic lights would be installed at Coloniale and Bouchard which would back up traffic coming from 50th St.

As a taxpayer living in Coloniale Estates, I want my community to remain residential. There is already 2 very large major commercial developments right across the street with a wide variety of retail stores. Additional retail in a residential neighbourhood being so close to the current developments is not warranted. I would suggest the city consider retail expansion in newer neighbourhoods on the west side of the city that is growing but has no retail development.

Thank you,

[redacted]

I am strongly opposed to this project as this area can hardly handle the traffic passing through this location as it is now. Cars parking on both sides of the street during soccer and kids scooting in and out between those cars. Add a strip mall to this and it will be an absolute catastrophe.

[redacted]

Hello,

My name is [redacted]. I am a resident at [redacted]. I am very concerned with the commercial proposal at 9 Coloniale Way. My concern stems from the potential traffic increase to an already very busy Rue Bouchard. This route is already used as a 50 St. bypass which has increased the traffic significantly. Adding commercial spaces with access along Rue Bouchard will only add to this problem. We have 2 schools in the area, a lot of our children walking around. The last thing we need is more vehicular traffic. The City has done very little to control traffic speeds along Rue Bouchard. The flashing lights at the cross walks are not even close to enough. I have never seen speed bylaw enforcement along Bouchard in my 12 years as a resident. I hope a traffic study regarding the potential traffic implications has been considered for this development.

Thank you for your time and consideration,

[redacted]

Hi

I am [redacted], homeowner at [redacted], Beaumont AB in Coloniale Estates.

I am against the development currently proposed at 9 Coloniale Way. The parking isn't enough for employees of that many businesses which means there's no parking for customers. The traffic to exit the lot would be left turns in either exit and would back up the 2 intersections along Coloniale Way.

I'm not sure why there needs to be 19 businesses... half of that might be reasonable, or a third of it. There's still vacancies in other buildings in Beaumont for businesses to lease.

My wife also mentioned that in the plans they have signage for a possible daycare but there's no outdoor space for a daycare to have an enclosed area for children and they can't just always use the park... so I'm not sure why they would even put signage for that. Nobody's going to send their children to a daycare with no fenced in play area.

I'm worried about what kind of businesses the developer wants to rent to if they're trying to jam so many places in a small property. Are we going to end up with vape stores, dispensaries, and liquor stores when there's already multiple across the street? The businesses must be pretty small if they're cramming that many in...so are they even going to be appealing to outside businesses to move to Beaumont if they don't have the required space?

I am glad that the Mayor posted this and the word is getting out for us to have a say. I hope that if this plan doesn't go forward, that we will be involved on future plan proposals as well. I want Beaumont to grow but in a way that makes sense.

[redacted]

I am writing to express my strong opposition to the proposed commercial development at 9 Coloniale Way, in our neighborhood in its current form. While I understand the need for thoughtful growth, the scale and design of this project do not suit our community and raise several major concerns.

First and foremost, the proposed development does not align with the character or aesthetic of the entrance to our beautiful golf course community. The density of 19 businesses on a lot that size feels completely out of proportion. This is not a large parcel of land, and yet the plan attempts to maximize usage far beyond what is reasonable for the space.

The parking situation is especially troubling. With number of parking stalls allocated, this is barely sufficient for employees, let alone patrons. Overflow parking will inevitably spill onto neighborhood streets, creating congestion, safety issues, and frustration for residents. We have already seen this parking issue arise from other poorly planned commercial and residential developments in our City and I do not want to repeat the obvious problem so close to my own home. Additionally, the increased traffic from employees, customers, and service vehicles will make entering and exiting the neighborhood significantly more difficult, particularly at peak times.

Look at where anyone would enter and exit the building? They would themselves have a terrible time getting out at that left turn and would likely lead to safety and traffic issues galore.

I am also concerned about the proposed daycare. The renderings show clear daycare signage, yet I see no dedicated outdoor play area appropriate for young children. Where is the fenced, safe outdoor space with properly sized equipment? The park beside this lot is designed for older kids and is not suitable for daily daycare use. Without a proper outdoor facility, I question how a daycare would even meet regulatory requirements—let alone attract parents to enroll their children.

Another issue is that the layout places the backs of all businesses toward our community. This means residents will be looking directly at service entrances, garbage disposal areas, and employee smoking or break spaces. This is neither visually appealing nor respectful to the people who live here.

I want to be clear: I am not opposed to commercial development altogether. I am opposed to this plan as it is currently designed. I would fully support a more suitable, scaled, and thoughtfully integrated development—one that respects the neighborhood's character, properly addresses parking and traffic flow, and includes realistic space planning for any proposed businesses, including a daycare with appropriate outdoor facilities.

I urge you to reconsider this proposal and work toward a solution that genuinely fits the needs and values of our community.

Thank you for your time and consideration.

[redacted]

Thank you for the opportunity to provide feedback on the proposed plans of 9 Coloniale Way.

I feel like this area would be good for a community raised garden project. It is walking distance for the people those areas and it would also be a way to keep those beautiful trees. I really hope with even a commercial development there that those beautiful trees will remain.

If it is to become a commercial area, I think that Beaumont should put a cap on the amount of vape stores, weed stores, liquor stores, nail salons and pizza shops. We definitely have more than enough of those businesses in Beaumont already.

A bowling alley would be great. Or an indoor playground for children.

I also wonder why we are building more commercial spaces when there are still several commercial spaces still open and empty?

Thank you for the opportunity to provide feedback.

[redacted]

Hello,

This is [redacted] with [redacted]. I grew up in Beaumont and now own a [redacted]. I noticed a recent Facebook post about the plans for 9 Coloniale way that had some interest regarding a community garden. I was hoping to make our services known in case this is something the city may be interested in.

Our greenhouses are hand made in our shop and are known for being extremely durable and designed specifically for prairie conditions. We also provide raised beds and have a 10 year warranty.

If a community garden is something that would be of interest to the city, we are offering community garden packages. Please let me know if you are interested. You can visit our website at [redacted] or give us a call at [redacted].

Thanks for your time!

[redacted]

Good Morning,

I am [redacted] living in Coloniale Estates at [redacted].

I strongly oppose the commercial development proposed at 9 Coloniale Way. The traffic at Coloniale Way and Reichert/Rue Bouchard is already congested during soccer games, school dismissal and when people are coming home from or going to work. The 2 way stop means that Reichert is typically moving traffic with right turns to the light and people waiting to turn left from Rue Bouchard onto Coloniale Way have a long wait even if its one or two cars waiting to turn left. Imagine when people are now exiting the development onto Rue Bouchard and getting backed up trying to turn left? And then to add to that, people trying to turn left onto Coloniale from the development close to the light. That property is just not positioned well for that volume of traffic. If the commercial plans was one or two doctors offices and not dozens of businesses I could see it being okay but if theres that many businesses, they will have way too much traffic in and out. The left turn from 50th street onto Coloniale will also be majorly backed up all the time because of the median past the light, only one direction of traffic can access the businesses from 50th... so residents and business customers will be in long lines of traffic waiting to turn left at the second most northern light coming into beaumont. Is the left turn lane even long enough to accommodate all those turning cars during rush hour or is beaumont going to get bottlenecked on the way into town?

To add to the traffic problem, there doesnt seem to be enough parking to accomodate daycare workers, cafe employees, doctors and nurses and admin staff and then factor in actual customers/patients. This development is just way too many businesses in a small space.

Lastly, the orientation of the buildings having their backs to the park and coloniale is just an eyesore. Youre telling me you want to take a beautiful golf course community that is one of the nicer neighborhoods in Beaumont and you want to turn the entrance into the backs of businesses with their exit doors and likely employees taking smoke breaks or taking out the trash being the thing we see as we drive into our once beautiful community? They couldnt have proposed this to be facing the other direction with backs of the businesses to the fences of the homes on Bonin Crescent because then they would have to eliminate that entrance and exit to their development for traffic... but If theres SO many reasons that they couldnt make this look better or make traffic work then maybe thats an obvious red flag that they're squishing too much in a small space.

The city of Beaumont has new development on the south end with vacant buildings. Theres new development up by Old Yale. There are also vacant businesses for lease in existing commercial parking lots that arent being utilized. Why do we need more space for businesses if our current spaces are still vacant? Cmon.

Thanks for listening. I strongly disagree with this development as it is proposed. If they want to do commercial and reduce it to A COUPLE doctor offices or a nice restaurant (no pizza, no vape store, no liquor store) then maybe they could gain our favour.

In regards to reference number 2025-077:

My name is [redacted] and I live at [redacted]. I travel past this vacant lot every day and I know there is a proposal to develop the area.

It is already a congested corner- eg. in the morning as people are trying to get to the school, or there are sporting events in the field. I would be deeply concerned about the traffic flow around an already congested area. It is not huge space so the thought of having 81 parking spaces and several businesses is difficult to imagine. While I am not opposed to developing the space, I would be opposed if the traffic flow issue is not addressed. What businesses are planning to go in? There are already several across the street so unless they bring real value and are unique to the area I don't understand how this would be at all convenient. I honestly would be more than happy if nothing happened to the space. I truly believe the traffic congestion will be a nightmare- my family moved to Beaumont and this area to avoid these problems. Please take this seriously and address these issues before further development.

Thank you for any consideration,

[redacted]

Good morning,

I am writing to formally express my opposition to the proposed development at Coloniale Way & 50 St. While I understand the need for growth and new business, this particular project raises serious concerns regarding traffic congestion and over-occupancy that would negatively impact the community.

- Traffic Impact: The proposed density would significantly increase vehicle flow on already strained roads. Current infrastructure is not equipped to handle such volume, leading to longer commute times, safety risks, and reduced accessibility for emergency services.
- Occupancy Concerns: The scale of this development far exceeds what the area can reasonably sustain.
- Community Character: Responsible development should enhance, not overwhelm, the community.

I urge the city to reconsider or substantially revise this proposal to align with sustainable growth principles. Smaller-scale projects, paired with infrastructure improvements, would better serve our city.

Thank you for considering my concerns.

Sincerely,

[redacted]

Hello,

I oppose this new development. I oppose it as there are already too many vacant commercial buildings in this city. I oppose it due to the limited road infrastructure especially during school times. I oppose it as commercial units should be kept in commercial areas of the city which there is plenty of empty land for this building to go other than this spot.

I think this city development team should be focusing on how this particular space could be used in a much more community oriented environment. Examples for this space could be community gardens, botanical gardens, a public picnic/bbq area such as the gazebos like in four seasons park. There also could be an outdoor gym for teenagers as this neighborhood has a large teenager population with nothing to do and nowhere to go. This park space could become a third space for people to be outside for those live in near it.

[redacted]

Good afternoon,

With respect to the project being proposed at 9 Coloniale Way (2025-077), I would like to raise a few concerns felt by many of us residing in the immediate area. In fact, I'm confident you will receive similar emails from several of my fellow residents. Let me begin by stressing that I am in favour of developing the area and believe that this lot has sat vacant for far too long. Like any Beaumont resident, I want to see our commercial tax base grow to help offset the burden on home owners as well as increase the amenities and services offered in the community. However, the scale of this particular design is what has myself, and many others, concerned.

Based on the depiction available through the city's website, this project will bring a minimum of 19 new businesses to that site. The resulting increase in traffic on Rue Bouchard and Colanial Way is a major concern. In fact, I do not believe this project should have an entry/exit point off of Rue Bouchard given that this street was never designed to accommodate such an increase in traffic volume. Immediately south of the proposed access point off Bouchard is a crosswalk that is already heavily used by elementary school kids and has been the subject of some 'close calls' given its location after a sharp bend in the road. Adding the traffic that would be expected from 19 new businesses would only exacerbate the safety issue.

Aside from increased traffic volume, my other concern has more to do with neighborhood feel and property value implications. Injecting a project of this scope onto an area that is effectively a residential neighbourhood feels like a missed opportunity for the city. Again, I understand that the investors for this project want to maximize their return, but adding this many units to such a small space feels very much like the Dansereau apartment complex that clearly dismissed neighbourhood liveability for developer profits. I know residents have suggested a community garden or park to fill the space, but I'm a realist and understand that this is not feasible. All that I'm asking/hoping for is that that the project be scaled back to account for the fact that this is a residential neighbourhood within close proximity to an elementary school that cannot accommodate 19 new businesses and the corresponding traffic.

Thank you for your consideration,

[redacted]

Hello,

My name is [redacted] and my address is [redacted].

I am opposed to the proposed development at 9 Coloniale Way.

The reasons for this are

- I do not want a business complex at the entrance to my neighborhood
- I do not want the added congestion this would create at the entrance to my neighborhood
- I do not believe a complex like this fits the small town feel that so many residents like me still want to hold onto
- This is right across from a soccer field/park/playground zone that is already very busy in the summer- a busy shopping centre creates danger for pedestrians and children

I believe this land should be left alone. And I would be happy to share more if you need to contact me.

Thank you,

[redacted]

Hello,

I am writing about the possible development in coloniale estates. This development would cause me anxiety about letting my children go to the park. At age ten I feel comfortable letting my son go to the park and cross the street by himself.

This proposed complex is large and would bring traffic that I would not be comfortable letting him do so.

I purposely bought in my neighborhood for the reason that it was safe for my children. This would change that.

Developments like this make Beaumont less desirable to live in. Long term it will negatively impact the whole town and it's reputation , in my opinion.

Thank you

[redacted]

Hello,

I think it looks to much like the south of town which is nothing like coloniale itself, the nice French architecture is not at all in this design. If they made it match coloniale estates better it would be better received but the residents, myself included.

[redacted]

I just wanted to express some concerns I have related to the proposed development at 9 Coloniale Way. I am a long term resident of Beaumont and the Coloniale sub-division.

While I understand and support the need for thoughtful community growth, I am worried about the suitability of this particular area for commercial use. The existing roads are already quite limited and were not designed to accommodate the increased traffic that a commercial development would generate. As it stands, the area experiences significant congestion due to the nearby soccer fields/park and the Coloniale Elementary school, particularly during peak hours. Adding commercial traffic to this mix will create safety concerns and reduce quality of life for local residents.

I respectfully ask that the City consider these impacts carefully and explore whether this location is appropriate for commercial development, or if additional studies and traffic mitigation measures are required.

[redacted]

Hi Sara, my husband and I are residents of Coloniale Estates. We understand there's an application (Ref #: 2025-077) to redevelop an unused parcel at 9 Coloniale Way into two CRU buildings with 81 parking stalls. This includes plans for stores, restaurants, and other businesses including a daycare.

We are vehemently opposed to this application for the following reasons:

- 1) This would be yet another unwelcome development squished into a relatively small tract of land. Traffic activity would be unreasonable, particularly with an elementary school in close proximity.
- 2) We do not need more basic retail. Montalet already has a very sizable shopping district right across the street. The type of CRU proposed attracts the same old pizza places, daycares, vape shops, liquor stores, nail salons, fast food, etc. This has already become a running joke amongst Beaumontonians.
- 3) There are several empty CRU units on the south side of Beaumont. The demand side for more retail is questionable at best.
- 4) A basic CRU like this one, even if it has French-style architecture, would ruin the character of Coloniale Estates. Personally, we're OK with paying a little more in taxes if it means maintaining the integrity of our community.

We understand there may be an argument for Highest and Best Use of the land, but it's time to put livability and quality of life first. This has been sorely lacking in Beaumont the last several years. No more of these CRU's please.

If this area must be developed, we suggest something that would integrate better into Coloniale Estates. Adult-oriented condominiums like Coloniale Gates are in demand, another development like this I think would be welcomed. Seniors care is also in high demand.

Another idea is a small boutique hotel. Beaumont needs one.

Mayor and Council are cc'd here, we are requesting they vote against this proposal.

Thank-you,

[redacted]

Good evening Sara,

I'm writing to express my concerns regarding the above proposed commercial development just inside the entrance to Coloniale Estates. While I understand the importance of business development in the City of Beaumont, I believe that this proposal would have a detrimental impact on the residential community as a whole.

Firstly - The size of the development, with parking spaces for 81 vehicles is too large for a residential neighbourhood. According to the plans, there will be only one exit point which would lead directly onto Coloniale Way between two already busy intersections (Coloniale Way/50th St and Coloniale Way/Reichert Drive/Rue Bouchard).

Secondly - A two story business/commercial development of this size inside the residential area (Traffic noise, pollution) would have a negative impact on homeowners and residents of Rue Bouchard and the surrounding area. I would hate to see the beautiful evergreen trees that are at the entrance to Coloniale Way be torn down to make way for rental spaces when there are many unused spaces already in and around Beaumont.

Will there be a public hearing at anytime for residents to attend and discuss/voice concerns?

Thank you for the opportunity to share my thoughts.

Sincerely,

[redacted]

I am opposed to this development based on the area is not properly able to handle the parking nor the increase in traffic. There is a reason this area wasn't designated as commercial originally.

[redacted]

Dear City of Beaumont,

- I have seen the proposed development plan for the space east of 50th street on Coloniale Way. This is a truly AWFUL plan in so many ways, but here are a few of my objections:

- It will completely ruin the overall esthetic of the entrance to one of Beaumont's most beautiful older neighborhoods.

- It is much too large/tall to back on to an established residential neighborhood! It's not like there will be additional commercial development going in alongside - it's only houses.

- The existing roads cannot accommodate the additional traffic this will generate. As someone who commutes down Rue Bouchard I cannot imagine how many accidents will happen due to cars turning on/off Coloniale at Rue Bouchard/Reichert Drive. Even if you put a 4 way stop there it will create havoc due to the sheer volume. That intersection is tricky enough with vehicles, pedestrians, cyclists and a school zone. There is a huge number of vehicles that turn onto Colonial Way from 50 St. to gain access to the whole neighborhood. Has a traffic study been completed? I shudder at the thought of closing access on or off those roads by putting in "traffic calming measures" as has been done in other areas.

- The homes to the south of this development will surely suffer a loss of property value. Who in their right mind would want to buy a home that backs on to something like what is being proposed?!

- We already have way too many of these ugly strip malls with massive amounts of available space to accommodate the type of business that invariably ends up in them. How many daycare/canabis store/nail salon/pizza place/liquor store combos does Beaumont really need?!

With respect, I kindly request that this proposed development plan be rejected and future proposals keep these concerns in mind.

Regards,

[redacted]

I am noting my concern as a citizen living on Renaud CRT regarding the above planned development...

I do not understand why we should incorporate a such an overwhelming building (s) into such a small parcel of land that doesn't call for this.. IMO this building would fit perfectly into the Reve area directly across from the commercial area already being built..

Beaumont looks like it's stuffing huge developments into small spaces! Are we really that short of commercial space?(It certainly doesn't seem like that looking at the empty office fronts?).

My biggest concern is the traffic safety, both car and pedestrian.. and in the past the town at that time was concerned about egress..

Am I seeing an entrance / exit East onto Colonial?/50th St N??!

With school , the park, and rush hour traffic, I believe the congestion along Colonial and Reichert Drives would require a traffic light, further backing up vehicles on N&S sides of 50 St. , including Montalet!

Looking at parking it would seem to be an absolute nightmare up and down the residential neighbourhood and park areas!

Unless we look at building on that park again!??

I'm really having a hard time fathoming why this type of development is required on this particular parcel of land!!!!!!???

And I do not support it!

I miss my town 🙄

[redacted]

To whom it may concern:

As a resident of [redacted] in Coloniale Estates, I am extremely concerned at the proposed development at #9 Coloniale Way.

Not only would a building that size be a complete eyesore in a residential area, but the existing traffic infrastructure simply could not handle the additional traffic.

The congestion from the proposed 81 stalls as well as delivery and waste trucks coming in and out at all hours, would definitely clog our 2-way stop intersection.

There simply is not enough space for 2 huge buildings in that small area. It would cast a shadow on the neighboring houses and will affect all of our house values.

It was bad enough to see multiple gophers run wild in the grass and crossing the streets without the city's attention. Now it's proposed to put up two 2-story buildings??? It's embarrassing to see that this is how our tax dollars are spent - to shove huge buildings in residential areas just to have them sit vacant like all the other new buildings that have gone up. All while ignoring public house values and public safety.

I suggest that you pave it and make a parking lot of it. In summer with the soccer/football field to the North, all the people park on the side of the road, restricting traffic views and congesting all 4 side of the streets. These people can use this parking lot and pedestrian crosswalk - which was finally installed last year. This parking lot could be used for farmers markets, food truck nights as well as things such as show/shine auto clubs. All things that would attract families, not drive them away.

I am simply beside myself that this has even gone as far as it has but I guess that its just one more example of how our little town of Beaumont is in such a hurry to expand into its 'Big City" trousers, that it is willing to overlook it's own taxpayer family neighborhoods for corporate dollars.

Why doesn't the city of Beaumont invest on a ring road to relieve traffic stress down 50th street? Put our tax dollars where it makes sense.

Shame on you.

Yours truly,

[redacted]

The proposed development 9 colonial way.

I do not support this development.

First off I do not believe the roads can handle the increase in traffic that will occur after this is open as that street and intersection can become extremely busy during school pick up, drop off and rush hour traffic.

Secondly I do not support putting large comercial buildings inside residential areas that back onto people's backyards, I believe they are an eyesore and will negatively impact the house prices nearby hurting people's investments.

Third these comercial business buildings go up and its the same old story more pot stores, liquor stores, maybe a daycare and most likely more places that will not be utilized. I believe these buildings should be in a more comercial area away from homes.

For this area I would rather it be turned into a green space or a space that could be used for outdoor activities, I feel as though beaumont is loosing its way as a small family community and is turning into just another big city and losing the thing that brought so many families to this area to raise children and more comercial buildings inside residential areas will make it worse.

There once was a time when the church at the top of the hill was the thing you saw before you reached beaumont and sadly now its the church on the hill next to a eyesore massive apartment building. This comercial building next to a bunch of houses will just add to that.

Please do not allow another large building to take away from people's investment or the appeal of the area.

Thank you for your time any questions please email back or you can call me anytime [redacted]

Attention [redacted]:

With reference to the proposed strip mall development in Coloniale Estates, I strongly disagree. There are numerous vacant spaces & buildings in the Business Areas that should be optimized rather than building another strip mall for what will be partially occupancy, more vape shops, liquor outlets.

Please, consider a green zone or an area to beautify the gopher infested lot.

Community garden plots, another soccer field, a spray park, a children's play ground.

Thank you for accepting citizens input,

[redacted]

Hello,

Emailing to share my concerns with the proposed development.

i have to express my displeasure with the project and hope it does NOT get approved. The insane congestion and unsafe conditions it would create for the community isn't something my family can support.

That space should be used for the community, develop the space as a green space. Trees, benches, picnic tables...nothing that would invite people to appreciate the space. We don't not need more commercial buildings OR apartments being built there. They're gross and distract from the soul of the community.

Please do not approve this.

Thank you,

[redacted]

The proposed development for the area to the right as you drive into Coloniale Estates does not seem feasible as there are simply too many units and there won't be enough parking in that small piece of land. As a resident that travels through Coloniale Way and Rue Bouchard every single day, I can say that if patrons have to end up parking on the streets, it will be too much, especially on days when there are sporting events at the field across the street. There are already so many people that park on the street which makes it tight to turn from Coloniale Way onto Rue Bouchard and vice versa. Please rethink this development and get rid of more retail space and turn it into a nice park or garden area perhaps.

Thank you.

Regards,

[redacted]

Good afternoon,

My name is [redacted], my family and I have lived in Colonial Estates for the past 14 years at #1 Radisson Court. My wife and I originate from Beaumont and after a short time away we decided to move back to Beaumont to raise our family, we chose Colonial Estates because it is, generally quiet and away from the noise of the city. We have always known that corner lot is a designated commercial zone, and we are ok with that. What we don't like is the mass size of the proposed build.

I feel where its location is and the community its located in; 2 stories is too high. The row of houses to the South will lose sun light and privacy, the row of houses to the East will also lose sun light and privacy. The traffic volume is also a huge concern. With this being the main route to New Horizon School, there is already so much traffic, pedestrians, kids walking and on bikes, scooters etc etc, plus its already used as detour for traffic to bypass 50th street.

Like I mentioned, commercial is ok, but a single story building would be sufficient, keep the larger buildings for the designated commercial sections of Beaumont.

Thank you for reading my email and hopefully taking our concerns into consideration.

[redacted].

Good afternoon,

My name is [redacted] and I am a fairly new resident of Beaumont (Colonial Estate's). I writing to you in regards to the proposed CRU that the city is considering in the gates of Colonial Estate's. This land lot should not be used to bring in retail units like vape shops, legal drug and convenience stores etc. There are dozens of empty CRU all over Beaumont that are zoned just for these such operations. This lot in Colonial Estate's should be used for parks, playgrounds, flower gardens, a community hall, and other such community amenities to make this area, community beautiful and vibrant. We all know what happens when we allow these crus into the neighborhood, road traffic increases, safety of our kids decreases, noise increases and no one what's retail signs lighting up their backyards. Please consider this email and many others I hope as a sign that Beaumont residential areas should stay residential.

Thank you and I hope to hear back from you

[redacted]

Attention: [redacted]

Development@beaumont.ca

Reference Number: 2025-077

I am writing in regard to Reference Number **2025-077**. As a resident of Beaumont and Coloniale Estates, I am concerned about the request for a development permit being proposed.

The proposal to amend the land use and construct two two-storey commercial buildings raises significant concerns. This area is already congested during events, particularly when soccer games are held in the field across from 9 Coloniale Way. During these times, children and parents struggle to find safe parking along the street due to limited space.

Introducing commercial buildings in this location will further increase congestion on 50th Street, Coloniale Way, and Reichert Drive. Additional vehicle traffic, combined with delivery trucks servicing these businesses, will create heightened safety risks for pedestrians and residents, especially children.

There is also concern about the potential negative impact on surrounding property values. This parcel of land has changed ownership multiple times over the past one to two years, which further raises questions about long-term development intentions.

My understanding is that the proposed development includes two two-storey buildings with multiple businesses. With that density, the available parking would be insufficient. The developers have suggested a proposed daycare; however, if that does not proceed, the commercial zoning would allow for other types of businesses, including liquor or cannabis retailers. This possibility is deeply concerning for residents due to additional noise, loitering and deliveries.

I would also appreciate clarification on the parking requirements under the applicable bylaws if a land-use change to commercial is approved and how the traffic flow will be addressed. It appears unlikely that the site could provide adequate parking for the scale of development being proposed.

Finally, I am concerned about whether the design, architecture, and landscaping of the proposed buildings would conform to the existing architectural controls and the overall character of Coloniale Estates.

For these reasons, I oppose the request for development permit and land-use change under Reference 2025-077.

[redacted]

My name is [redacted] and I live at [redacted] with my wife and two daughters.

We have been looking forward to seeing what will be done with this land since we moved to Beaumont over 5 years ago and we are very excited to finally see something done.

The following are concerns we have with this:

1. Will the design of the building match the french style of architecture and history of town, similar to the design of the building that houses Chartier, Jefs and Maina's right downtown.
2. What businesses will be in the buildings? It would be great to see a coffee shop, or even a play place here for kids (Daycare is a good). Something a little different than what seems to be in every commercial space in town.
3. The traffic on Rue Bouchard is already very busy, It will only get worse with this. What type of Traffic / Speed Control will happen on Rue Bouchard? It would be ideal if there are speed bumps or a couple stop signs along the way to reduce speeders more specifically on this road.

Again, very excited to see this space come to life!!

Thank You

[redacted]

Position on Proposed Development: Against this development

Hello,

I have been a resident of [redacted] for over 11 years, and my property backs onto the land in question. After reviewing the proposal, I wanted to share some concerns that I believe deserve careful consideration.

1. Size of the Development

A project of this scale should have been part of the original planning for Colonial Estates. Introducing such a large commercial space now will significantly alter the character of our community. Because of the Gas line that runs parallel to my fence this project is backed onto Colonial Way, the light pollution will drastically change the way people enjoy their yards and produce noise that will not dissipate in open air as it would deflect off the building s towards the neighborhood.

2. Traffic and Safety

Currently, there is only one main exit from Colonial Estates (turning left onto Rue Bouchard) if someone is leaving this development directly, the alternative is to route through the community. Adding commercial traffic will create congestion at an already busy intersection. This area sees heavy pedestrian activity—children walking to school, families during sports seasons, and kids biking in summer. In winter, the added traffic could create unsafe conditions for pedestrians and introduce the high probability of collisions from impatient drivers. The traffic patterns shown on the documents show no way to exit without entering into the community, this should be a deal breaker with a development so large.

Past Concerns and Current Impact

This property has been problematic for years, from unmanaged gopher issues to previous attempts at hosting large religious events without regard for residents. Recently, the removal of grass behind our homes felt like an effort to make the land appear neglected, possibly to justify development.

I understand the need for growth and the potential tax benefits of development. However, Beaumont's charm lies in its sense of community. Every project should add real value for residents—not just financial returns. My request is that council carefully review all aspects of this proposal and ask: *Does this make life better for the families of Beaumont? Does this impact the residents of Colonial Estates negatively? Is a project of this size with this much parking add risk to families that access the park across the street the rink at the school?*

Thank you for your time and consideration. If there are any questions, please feel free to reach out

Sincerely,
[redacted]

Attention [redacted]:

I'm writing to express my deep concern about the proposed development permit and land use change at 9 Coloniale Way.

Our neighborhood is already struggling with traffic congestion, and the plan for two commercial buildings with 81 parking stalls would intensify this problem to an unsafe level. It's hard to imagine how our family-oriented streets could possibly handle the added vehicles without putting people at risk.

Directly across from the site is a field that becomes a gathering place for children and parents every spring and summer. Soccer games, football practices, families walking to and from activities - it's a place where our community comes to life. Only a block away is an elementary school where so many of our children walk to every day. Introducing commercial traffic into this environment feels not only impractical, but dangerous. These are children - our children - whose safety would be threatened by a dramatic increase in traffic.

Beyond traffic concerns, converting this land to commercial use, brings the likelihood of more noise, litter, and crime. This isn't just a matter of aesthetics; it affects the sense of safety and peace that drew many of us to this neighborhood in the first place. It also puts the long-term value of our homes and community at risk.

I understand the desire to develop this space, and agree that something should be done with it, but commercial development is not what this neighborhood needs. I urge the city to consider purchasing the land and creating something that truly enhances our community - a park, a shared green space, or another project that supports the families who live here, rather than putting them in harms way.

Our neighborhood deserves development that strengthens it, not development that compromises the safety and wellbeing of the people who call it home.

Best Regards,

[redacted]

To whom it may concern;

Hello , my name is [redacted], I am a resident of Beaumont residing at [redacted].

I would like to that the space already has looked hideous all summer. It used to be well maintained, and I don't know who owns it anymore. I know there's rumors that there was going to be a Sikh temple there, but obviously no one has been taking care of that spot. In regards to developing into a commercial space I think that Beaumont already has enough commercial spaces that are empty. I've been in Beaumont for eight years and it's grown so much and it's been expanding out on all sides with farmland into housing and everything, but I don't think that we need to be adding more commercial spaces into the core part of Beaumont where it's already so small and congested. How would that even work to have a commercial space there like that would be even so much traffic the infrastructure is not there for that? Coming through that road already when there's games going on at the field across the street is so congested and it's so hard to even drive home when the street parking is all of the people at the fields. I can't imagine how much crazier it'll be with commercial spots there. And as I said how many empty spots do we already have in Beaumont with no businesses in it so why are we continuing to let developers build more buildings that are empty. Beaumont is starting to lack green space for families and it's a very family oriented place the population here is very young. Again I don't know who owns it. I know that mattamy used the design center there so I don't know if it was Beaumont that owned it and now they've sold it but it's very disappointing that there is a proposal to put a commercial space there where if beaumont owned it, they could've done something with that space for the community instead.

Regards,

[Redacted]

I'm providing some feedback & our thoughts about the proposed rezoning & commercial development of the vacant lot on the corner of 50th St., (9) Coloniale Way, Rue Bouchard & Reichert Drive.

We have many major concerns about the proposed land use for this location

This site is a **very busy area/intersection** - to allow the proposed rezoning/commercial development would be a huge mistake!

1. Access to this site, both entry & exit would result in a major fiasco with safety issues & concerns! The **congestion & traffic flow in & out of Coloniale Estates** would be greatly impacted especially since 50th Street entry to this area is major, one of only two accesses in - we just can't imagine the congestion that the development of this space as a commercial/retail area of this type would be in the best interests of Coloniale Estates. The intersection on 50th St. is dangerous and has already seen many accidents!

The traffic back up would be unreal for anyone trying to turn onto Coloniale Way (which is a major access to so many other off shoots/roads into all the neighborhoods in Coloniale) from the North & coming across from from the W (Rue Montalet) & somewhat coming from the S as there already is a turn off lane onto the site! Rue Bouchard access would be horrific!

2. New Horizon School traffic & activity would be greatly affected incl. safety issues! The traffic flow during school hours is always very busy!

3. This location is across from a park & a playground, again, safety issues!

4. This location is not appropriate for this type of commercial development - there is going to have to be another exit & entry being that there is such a short distance of access;

5. The proposed businesses are just **more** of the same that we already have in Beaumont, we do need development but surely this prime location could be suited for some better use or need.

6. This type of development would have a negative impact on the surrounding residential area, it's kind of **right in the neighborhood!**

7. There is the potential that this type of retail development could have a negative aspect in regards to property value in the immediate neighborhood!

8. Residents will be **forever impacted** by this congestion, the frustration of coming & going caused by this type of development feels like a huge negative concern! Coloniale Way is a major entry/access to many other neighborhoods as you drive in, the traffic is already busy enough! There is also an area backing Coloniale Golf Course being developed right now so the traffic will increase even more when the homes are sold! It's not reasonable or appropriate to have to use Range Rd 241 to avoid congestion, it'll just cause issues at 241 & Country Club Drive/Coloniale Way!

If this rezoning is allowed for this type of development, the negative aspect/affect will be felt immediately and there's no going back!

Please consider all the feedback from the residents in this area and what the long term fallout would be!

Thank You for your time,

Regards,

[redacted]

My name is [redacted], I live in the Colonial Estate area at [redacted].

I am sending this email to let you know I am strongly opposed to this plan reference number 2025-077 to change from residential to commercial in this area of town. This area is residential with a school close by. If the town approves this change it will impact residents in the community. The infrastructure in this area is not set up for commercial space and cause issues with access and traffic.

I am not aware of any areas in town that would have this much commercial impact to residents.

There is lots of available space in other parts of town to accommodate this type of commercial property.

Please have the town contact me for discussion and input into this change.

Regards

[redacted]

To Newly Elected Council;

In opposition of this development;

1) Increased traffic & noise to the neighbour, keep commercial development away from residential areas, don't follow big city thinking

2) Previous council made radical decisions with no consideration other than for income then spend it irrationally - let's not go there again

Regards,

[redacted]

With respect to the project being proposed at [9 Coloniale Way \(2025-077\)](#), I would like to raise a few concerns felt by many of us residing in the immediate area. In fact, I'm confident you will receive similar emails from several of my fellow residents. Let me begin by stressing that I am in favour of developing the area and believe that this lot has sat vacant for far too long. Like any Beaumont resident, I want to see our commercial tax base grow to help offset the burden on home owners as well as increase the amenities and services offered in the community. However, the scale of this particular design is what has myself, and many others, concerned.

Based on the depiction available through the city's website, this project will bring a minimum of 19 new businesses to that site. The resulting increase in traffic on Rue Bouchard and Colaniale Way is a major concern. In fact, I do not believe this project should have an entry/exit point off of Rue Bouchard given that this street was never designed to accommodate such an increase in traffic volume. Immediately south of the proposed access point off Bouchard is a crosswalk that is already heavily used by elementary school kids and has been the subject of some 'close calls' given its location after a sharp bend in the road. Adding the traffic that would be expected from 19 new businesses would only exacerbate the safety issue.

Aside from increased traffic volume, my other concern has more to do with neighborhood feel and property value implications. Injecting a project of this scope onto an area that is effectively a residential neighbourhood feels like a missed opportunity for the city. Again, I understand that the investors for this project want to maximize their return, but adding this many units to such a small space feels very much like the Dansereau apartment complex that clearly dismissed neighbourhood liveability for developer profits. I know residents have suggested a community garden or park to fill the space, but I'm a realist and understand that this is not feasible. All that I'm asking/hoping for is that that the project be scaled back to account for the fact that this is a residential neighbourhood within close proximity to an elementary school that cannot accommodate 19 new businesses and the corresponding traffic.

Thank you for your consideration,
[redacted]

Good morning,

Thank you for the opportunity to provide feedback on the proposed development of the vacant lot on the corner of 50th street and Coloniale Way.

Whilst I understand and do not stand in the way of growth, I really have to wonder about the intent of developing this lot into yet another strip mall, with the possibility of not being able to fill the storefronts with anything other than a vape shop, a liquor store, a fast food joint, or a nail salon.

Added to this the fact that the field across the way is used for football practise in the summer, and it is already painful to drive down this road on any given night of the week, I wonder what the addition of this mall with another 90 stalls would bring to the community. Respectfully, it would likely be better served as a dedicated parking space for the myriads of people who use this field in the summer for their activities.

If there is no other way forward than absolute development of a strip mall, I implore you to consider the following:

- Please do not let the backs of the stores face the road or the field – this would be an eye sore, and I am sure the people in their backyards barbequing do not want to be faced with watching people going into and out of shops all day. Consider making the store fronts face the field
- Consider cramming in LESS store front and MORE parking spaces, so it can be utilized multi-functionally
- Please, for the love of everything good in this world, put a discretionary letting policy or buying policy on the developers that would LIMIT what they could use the properties for. We do not need more of the above; we need more offices for clinics, daycare, etc. Do a community needs assessment before you just let it all be.
- Please let them build in the style of the EXISTING Coloniale, and not the “modern” style across the road. Consider something more along the lines of what was developed on main street/the registry.
- Please consider it being single storey buildings ONLY

Thank you in advance for your respectful considerations in terms of the development in our neighborhood.

Kindly,

Hello,

I live on [redacted] not far from this, I would like to give my feedback on this project.

I feel that using it for commercial makes sense, especially for how close to 50th it is - HOWEVER - the parking seems horrendous.... the houses around their will suffer. Getting in and out of Colonial is already a bit congested during school hours, I cant imagine those stop signs. Anything being proposed that there is "street parking" should immediately be rejected. After what happened last year with the condos and daycare West Beaumont - I really hope you are being vigilant on abusing the rules and by laws here.

Please consider making that a 4 way stop and making **STRONG** rules on required parking **PER** store.

Thank you for your time.

[redacted]

To Whom It May Concern,

I am writing to express my concerns regarding the proposed development for the lot located at **9 Coloniale Way**. After reviewing the concept, I believe the project, as currently planned, raises several practical and safety issues that warrant reconsideration.

Traffic Congestion & Safety

The proposed development is sizeable, and the traffic impact is a major concern. Coloniale Way already experiences steady flow, and adding a multi-building complex will only intensify congestion. More importantly, the proposed entrance and exit sit only a few feet away from a major two-lane intersection. This placement is likely to cause significant traffic backup and create unnecessary safety risks for drivers and pedestrians alike.

Inadequate Parking

According to the site plan, five parking stalls are shown on each adjacent road (Coloniale Way and Rue Bouchard). However, based on a realistic assessment of available space, these stalls simply **cannot** fit as drawn. It is unclear whether proper measurements were taken, but the plan appears to overestimate what can safely and legally be accommodated. With multiple buildings proposed, the parking allocation falls far short of what is needed.

Concerns With Changing the Zoning to Commercial

Changing the zoning designation to commercial at this stage will make it even more challenging to work collaboratively with the developer to create a project that has a positive, lasting impact on the community. Once a commercial designation is granted, it limits flexibility and reduces opportunities for meaningful input into future plans. This risks locking the City and residents into a development style that may not reflect the needs or character of the surrounding neighbourhood.

Overdevelopment & Community Character

Beaumont already has a generous supply of strip-mall-style developments, and residents are increasingly concerned about maintaining the character and accessibility of our community. At this point, adding yet another cluster of buildings with inadequate infrastructure feels less like thoughtful planning and more like Beaumont collecting strip malls the way some people collect snow globes.... except ours don't fit on a shelf nearly as nicely.

For these reasons, I respectfully request that the City reconsider the proposal in its current form and require revisions that address traffic flow, parking feasibility, zoning impacts, and overall community benefit.

Thank you for your attention to this matter. I appreciate the City's ongoing efforts to balance growth with the needs and safety of residents, and I hope these concerns will be taken into account during the review process.

Sincerely,

[redacted]

Good day [redacted],

Thanks for reaching out concerning 9 Coloniale Way. My information follows:

[redacted]

Resident of this address since 2017, Beaumont since 2008.

Please do not develop this space as has been presented. My reasoning follows:

1. This is primarily a residential neighbourhood, with LOTS of foot traffic, including kids. The intersection of Bouchard and Colonial Way is VERY busy, as an inlet to the densely populated Coloniale neighborhood. Adding in a commercial space at the very entrance to this neighborhood will result in congestion in / out, and will also present traffic safety risks to the MANY pedestrians.
2. How will traffic flow in and out of this development? Presumably, onto either Coloniale Way or Bouchard. Those making left turns out of the lots, onto either of those roads, will create increased traffic congestion. This area is already experiencing high volumes of traffic.
3. Bouchard is a speedway as it is. I once asked that a radar device be installed on Bouchard, near Brochu court, to slow down traffic that comes whipping down the hill and around the corner. If you add a commercial development, you will increase the quantity of vehicles using Bouchard as a means of entry. Again, I remind that this is primarily a residential neighbourhood.
4. Commercial developments within Beaumont are primarily in the direct vicinity of 50th street. The outskirts of Beaumont feature almost NO walkable commercial development. Move this outside of existing neighbourhoods, and build AROUND it.
5. Light and noise pollution. When McDonalds, Scotiabank and 7-11 were added, the area at the North end of Beaumont became loud, noisy, and lost all sense of quiet residential. There is no longer any calm or serenity walking through Coloniale Park. Please do not make this worse by further densifying commercial development into the space.
6. This development is an eyesore for those on Bonin who are backing on to it. Gone is the clear skyline, added now is the back of a building. It's unfair for these homeowners to have this change made to their view when moving into a development that is 30+ years old.

Please do not do this.

[redacted]

Hello,

I am writing to voice my concern for the proposed development at 9 Colonial Way. My family does not agree with the proposal. The new traffic that will come in with the new businesses poses a large safety hazard. With the school only a few blocks away, and the large amount of traffic that is already present during every sporting event in the field across from the proposed site, it would be a disaster. There are very young children that cross that intersection when school is beginning and ending. There are many buses. And during sporting events, cars are already parked for several blocks down Reichert Drive - it can be chaotic already, we cannot imagine what it would be like with all the traffic for so many businesses.

A scaled down version of the plan might be more reasonable, hopefully to create a space for something like a doctor's office (much needed here in Beaumont). But a multi unit, 2 story, large scale parking lot, is not wanted in this area.

[redacted]

Dear [redacted],

Here are some of the concerns we have about the proposed changes of 9 Coloniale Way from Integrated Neighbourhood to Commercial District.

First and foremost this was never meant to be a commercial area. Why would this little parcel of land be changed to commercial when everything around it is residential and anything commercial is nicely located on the west side of 50 Street? We were one of the first people to move into the crescent which backs onto this piece of land. It was zoned for low traffic businesses and/or businesses that close at a decent hour in order for it not to interfere with the residents. We are not opposed to the development of this property, we are actually happy to see it being developed, but here are our concerns:

1) The congestion of traffic this would cause on this corner would be awful. Colonial Way and Rue Bouchard were never meant to have a steady flow of traffic. The increased flow will not only be noisy but also dangerous. There are a few school bus stops for young kids within a block of the proposed new entrance off of Bouchard. There are also mailboxes within meters of the same proposed entrance. There are already issues with turning from Bonin onto Bouchard and from Bouchard onto Coloniale so adding an additional two lanes of traffic (one going into the new development from Bouchard and one coming out onto Bouchard) would be hazardous. At times we can easily wait 10-15 minutes because of the traffic coming from the east and west on Coloniale Way and going north from Bouchard and south from Reichert. It is dangerous for vehicles to cross at times never mind the children who are distracted walking to school or rushing to catch their buses. If you are in the area during the warm school months you will see kids every where not really paying attention specifically before and after school hours.

2) With having an access road way immediately behind our fences we will be exposed to more environmental and noise pollution. There is a slight buffer now because of the distance from 50 Street and Coloniale Way but with only a few meters between our houses and the driving lanes/parking spots we will be exposed to a continuous flow of fumes. This will be especially bad in the winter when people are leaving their cars running to keep warm. How would you like that? Could a barrier wall be built for us to provide a little protection? Not sure random trees will help this issue. Could the back of the units be flipped so that the back of the businesses are against our fence and the cars are on the other side of the buildings? This would also aide in the noise pollution. No other neighbourhoods in Beaumont have allowed this vastness of businesses to be built so closely to residents. Why would it be tolerated here?

3) Light pollution. A second storey of businesses would mean lights higher up which would in turn be shining right into our bedroom windows which would be facing this project if they are not backing onto us. Light pollution is a real problem. If there are no lights how will vandalism to adjoining properties or loitering at all hours of the night be prevented?

4) The buildings backing onto Coloniale Way will look out of place. We like the design of the buildings but the positioning seems awkward. It will make this area look aesthetically unpleasant and as thrown together. Is that what we want people to see as they enter Beaumont? We feel the south side industrial part of Beaumont already looks and is disorganized and thrown together without thought. Can we please keep this area flowing and aesthetically pleasing? Is it not easier for people to find things if all businesses are in one area (on the west side of 50 Street) instead of randomly throughout Beaumont?

5) Lastly, Green Space Alliance has just recently hand delivered notices to the nearby property owners. The notices were dated November 18 but we did not receive them before November 25. Ironic since that means less time for people to write letters before Dec 5 which is the deadline for sharing our opinions. What else will this developer and owner of the land not honestly disclose? According to the plan that was submitted to the town there was no evidence of any areas where there could be drive-thru facilities. In our notices (from GSA) they specifically

mention that drive-thru facilities are an option in this space. It also mentions restaurants so does that mean there could be a restaurant patio operating at all hours of the night just a few meters away from our homes? Is there a way to ensure that there won't be polluting drive-thrus or noisy businesses in this complex so that those who actually live nearby can continue to live in peace? According to the GSA website they say that they do full environmental, social, cultural, and economic reviews BEFORE proceeding with any project. Where are these reviews? They state that their projects are environmentally responsible, culturally vibrant, technologically progressive, economically feasible and people oriented. Could they explain how they meet this criteria in this case?

We hope that you reconsider accepting the new proposal. We feel keeping the status of Integrated Neighbourhood would be best but if changing it to Commercial District is chosen we sincerely hope that the above concerns are considered.

Thank you for your time.

[redacted]

PS. Since writing the draft of this letter we have had the opportunity to discuss these issues with GSA. We were informed that there will not be any drive-thrus or noisy restaurants with patios in either complex due to space limitations. The bays as presented in the proposal would not be combined to make bigger areas as would be needed for a large restaurant or other large businesses. It would be more professional type services. When asked if we could see the results for the reviews they've completed prior to requesting this land-use change they've stated that the letter we received was part of that process. This was interesting since their website states that they do full environmental, social, cultural, and economic reviews BEFORE proceeding with any project. I think it is clear why we are hesitant with this whole project. The people we spoke with at GSA were very professional and answered our questions but we do see a few red flags. Although I would like to trust the process we feel our above concerns are still valid.

Hello,

I do not support this proposed development.

I am a resident living in Coloniale Estates for 27 years and I believe this is a poor fit for the original vision of this subdivision.

This proposed development is a monstrosity that is out of place. These developments are happening all over Beaumont and there is no need to build another one regardless of how delightfully pleasant a façade is promised. This scale of development is more appropriate in new (green field) development regions, not existing subdivisions.

I do support the development of the remainder of the subdivision as originally envisioned and documented in the neighborhood structure plan re. low density detached single family residential, a small multi family development (something modest, no more than 2 story similar to Coloniale Gate) and the commercial development (something modest that will be a better fit for the neighborhood).

Perhaps the developer will say they need to build this large or the development won't be financially possible - if that is the case, development should wait.

Changing land use bylaws after people buy into / invest in building a home in a subdivision is incredibly disrespectful.

Since work has already begun on the site, I believe the decision has been made and this is more Beaumont telling residents what they are getting - like it or not (aka "consultation").

Thank you,

[redacted]

Hello

I oppose the proposed changes to the land at the entrance of Coloniale Estates. The area does not have adequate roadways for the development as indicated. The impact on the feel of the neighborhood as well as the impact pedestrians considering the amount of children in this area attending schools. Additionally across the street is a sports field and park that seasonally impact traffic flow and parking.

A development of this size does not fit with the established neighborhood and is ill conceived at this time.

[redacted]

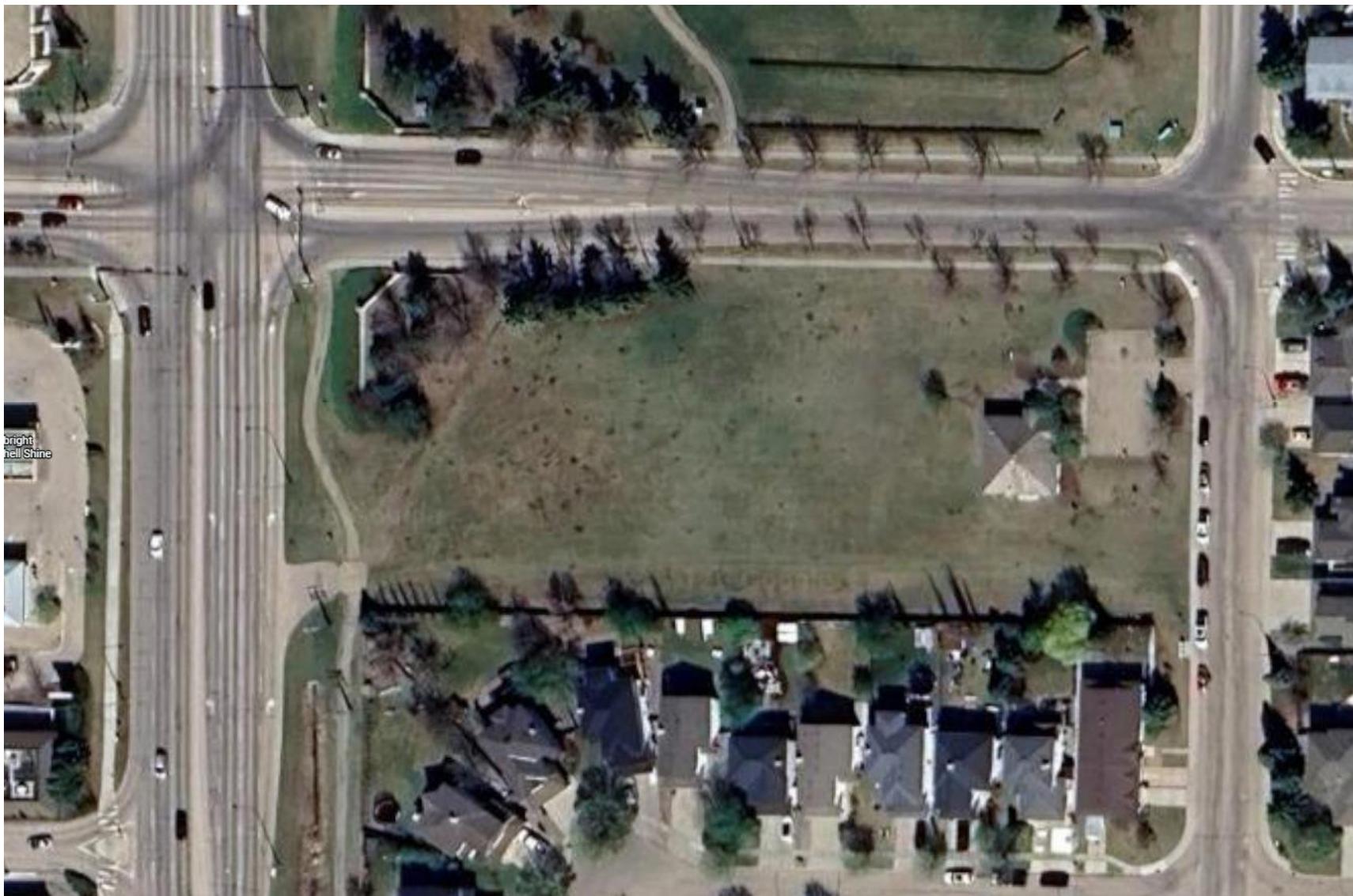


Figure 1 Aerial Photo. Source Google Maps



Figure 2: Looking east from 50th Street (Google Streetview)



Figure 3: Looking South/Southeast (Google Streetview)



Figure 4: Looking Southwest from Coloniale Way (Google Streetview)



Figure 5: Looking Southwest from Coloniale Drive (Google Streetview)



Figure 6: Looking West from Rue Bouchard (Google Streetview)



SDAB-26-01
2025-077
TWO 2-STOREY COMMERCIAL
BUILDINGS

LOCATION 9 COLONIALE WAY



Integrated Neighbourhood
(IN) District

| | | | | |
|--|--|----------------------------|-------------|---|
|  <p>City of Beaumont 5600 49 Street Beaumont, AB T4X 1A1</p> | 9 Coloniale Way | | | <p>Legend</p> <p> Location:</p> <p> Registered Parcels</p> |
| | 9 COLONIALE WAY | | | |
| | Application No. 2025-077 | Mapped By: Craig Thomas | Checked By: | |
| | Numeric Scale 1:4649 | Date February 10, 2026 | | |
| | Projected Coordinate System CANADA NAD 83-3TM 114 | | | |

WHAT THE APPLICANT APPLIED FOR:

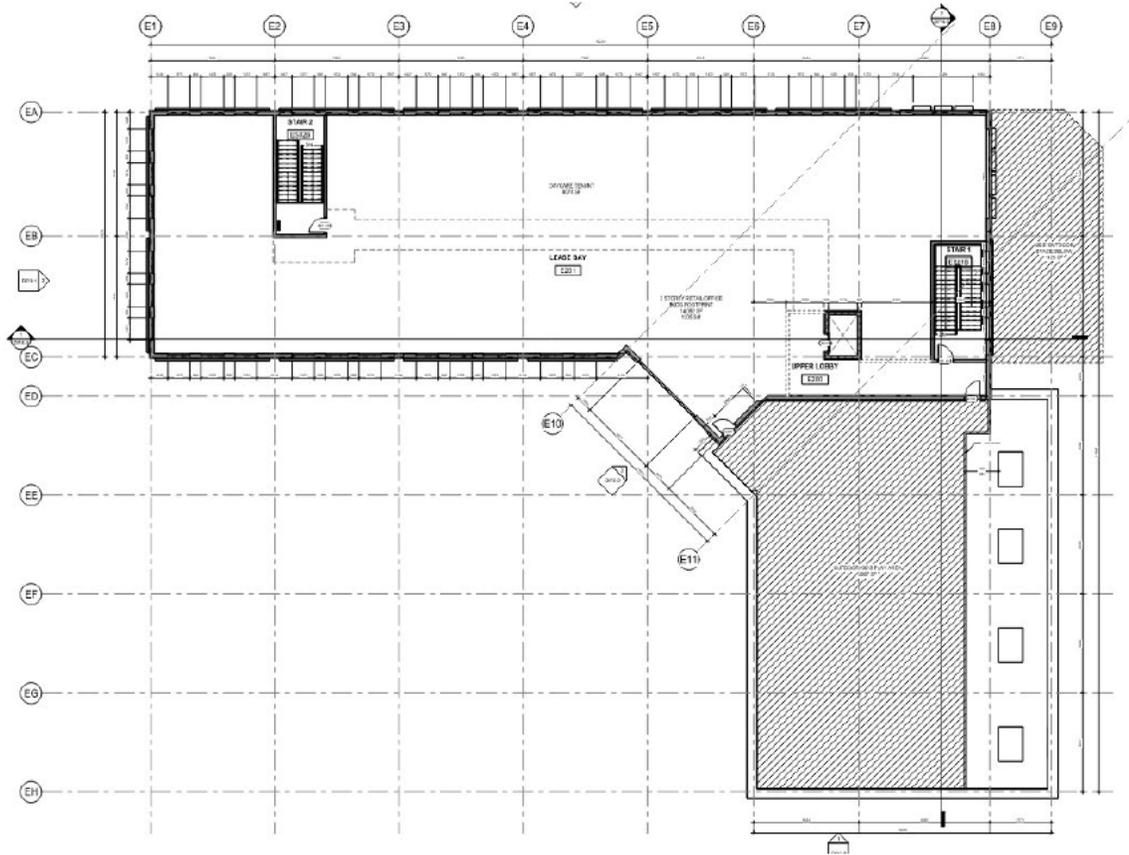


June 25, 2025, Development Permit Application

“2 Two Storey Commercial (CRU) buildings (shell only) to be constructed – East building proposes daycare use on upper floor with retail and service – general + restaurant/café uses below.

West building proposes office use on upper floor with retail and service – general + restaurant/café uses below”

WHAT THE APPLICANT APPLIED FOR:



APPROVED DEVELOPMENT PERMIT

January 5, 2026 – Development Authority Authorized the following:

Buildings

- Two – 2 Storey Buildings with a total of 4,993 square metres
 - 2,152 square metres (east building) and 2,841 square metres (west building)

Uses

- “Education” a Discretionary Use
 - 750 square metres on the 2nd floor of the east building
 - 420 square metres of play area
- “Restaurant/Café” a Discretionary Use
- “Retail & Service – General” a Permitted Use

USE CLASS DEFINITIONS

Education – “development of public and private places of learning for any age including licensed childcare facilities and/or research facilities.”

Restaurant/Café – “development where prepared food and beverages are offered for sale to the public for consumption on-premises or off-premises.”

Retail & Service – General – “a development that provides goods or services directly for sale to the consumer, and where such goods or services are available for purchase on the premises and can include cannabis accessory.

Outdoor display that does not hinder pedestrian movement may be permitted.”

LAND USE BYLAW CONTEXT

Zoning: Integrated Neighbourhood (IN) District

General Intent:

“To provide a variety of housing options with integrated local commercial and business opportunities, primarily in greenfield areas”

Permitted and Discretionary Uses (Section 3.4.2)

- “Education” and “Restaurant/Café” are classified as Discretionary Uses
- “Retail & Service – General” is classified as a Permitted Use

LAND USE BYLAW CONTEXT

Permitted Uses: applicant is entitled as of right.

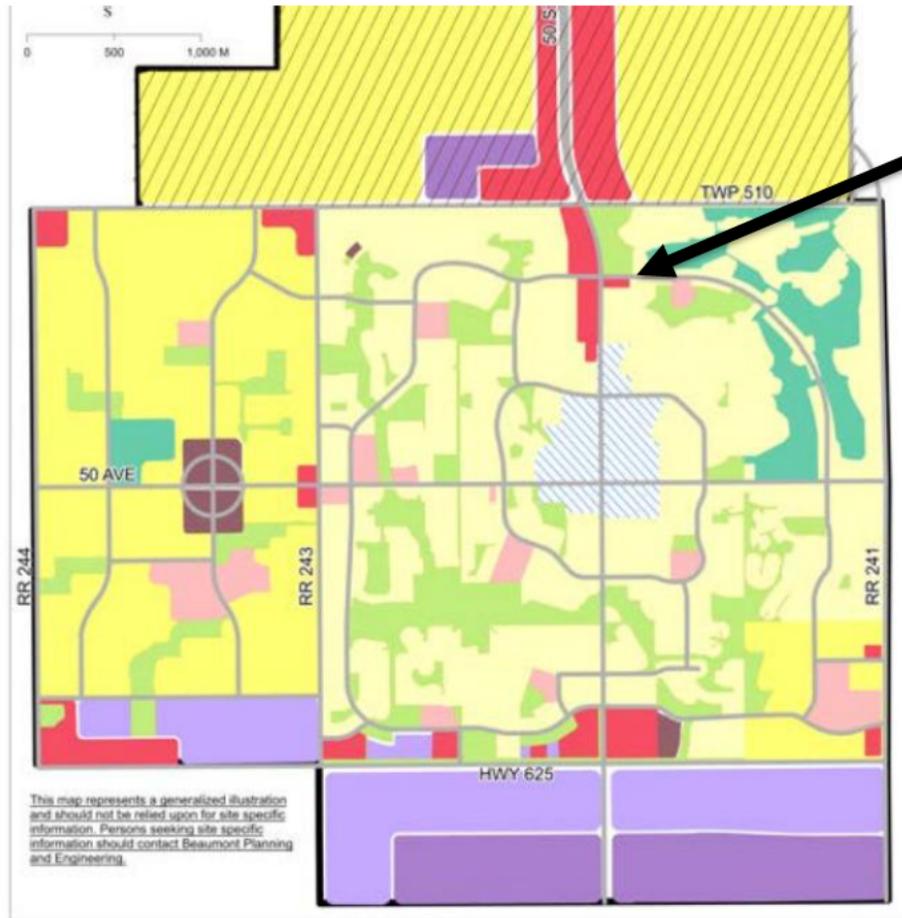
Discretionary Uses: permitted at the discretion of the Development Authority

- Considered generally appropriate in the district
- Are of such a nature they may or may not be reasonably compatible with neighbouring uses, depending on the circumstances.
- Needs to be significant “planning reason” to take the applicant’s right to the use away.

DEVELOPMENT AUTHORITY'S ROLE

- Apply the Development against the Land Use Bylaw
 - Discretionary Uses
 - No Variances
- Apply Statutory Plans where relevant
- Assess Reasonable Compatibility of Discretionary Uses
- Consider technical information and design compliance
- Consider public feedback
- Exercise planning discretionary within authority to do so

MUNICIPAL DEVELOPMENT PLAN



Identified as “Commercial”

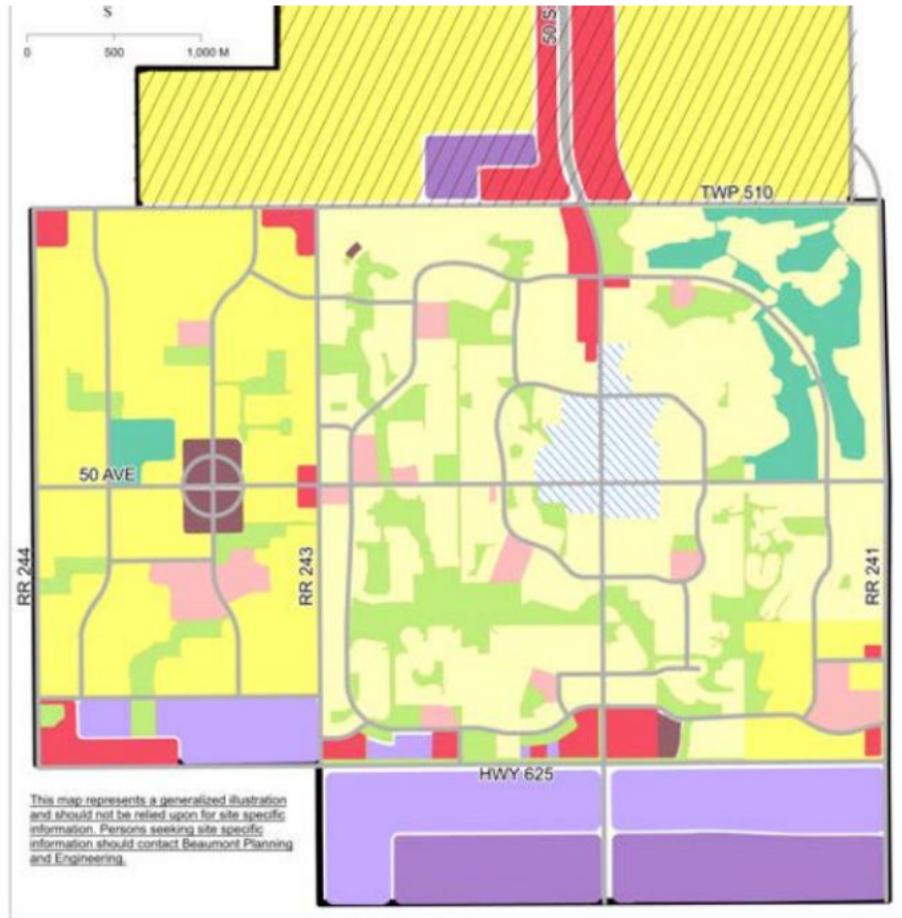
Commercial

Commercial uses for areas identified on **Map 9 – Land Use Concept** include concentrations of retail, office, and entertainment uses, including large-format stores that attract a community-wide customer base. The policy area also includes a broad range of commercial uses that rely on

highway and major corridor access and exposure, such as auto services, sales and gas stations, hotels, restaurants, and other hospitality services. These areas are designed to take advantage of key transportation routes and integrate highway commercial opportunities.

- | | |
|--|---------------------------|
| Legend | Future Growth Area |
| Future Residential | Centre-Ville |
| Established Residential | Light Industrial |
| Commercial | Private Recreation |
| Business Park | Parks & Open Space |
| Institutional & Public Facilities | Major Roads |
| Community Commercial / Employment Centre | Municipal Boundary |

MUNICIPAL DEVELOPMENT PLAN



Section 4.13.4

This policy encourages commercial buildings to be oriented toward public roadways to help establish a pedestrian-supportive streetscape. The proposed development reflects this intent by addressing the public street interface.

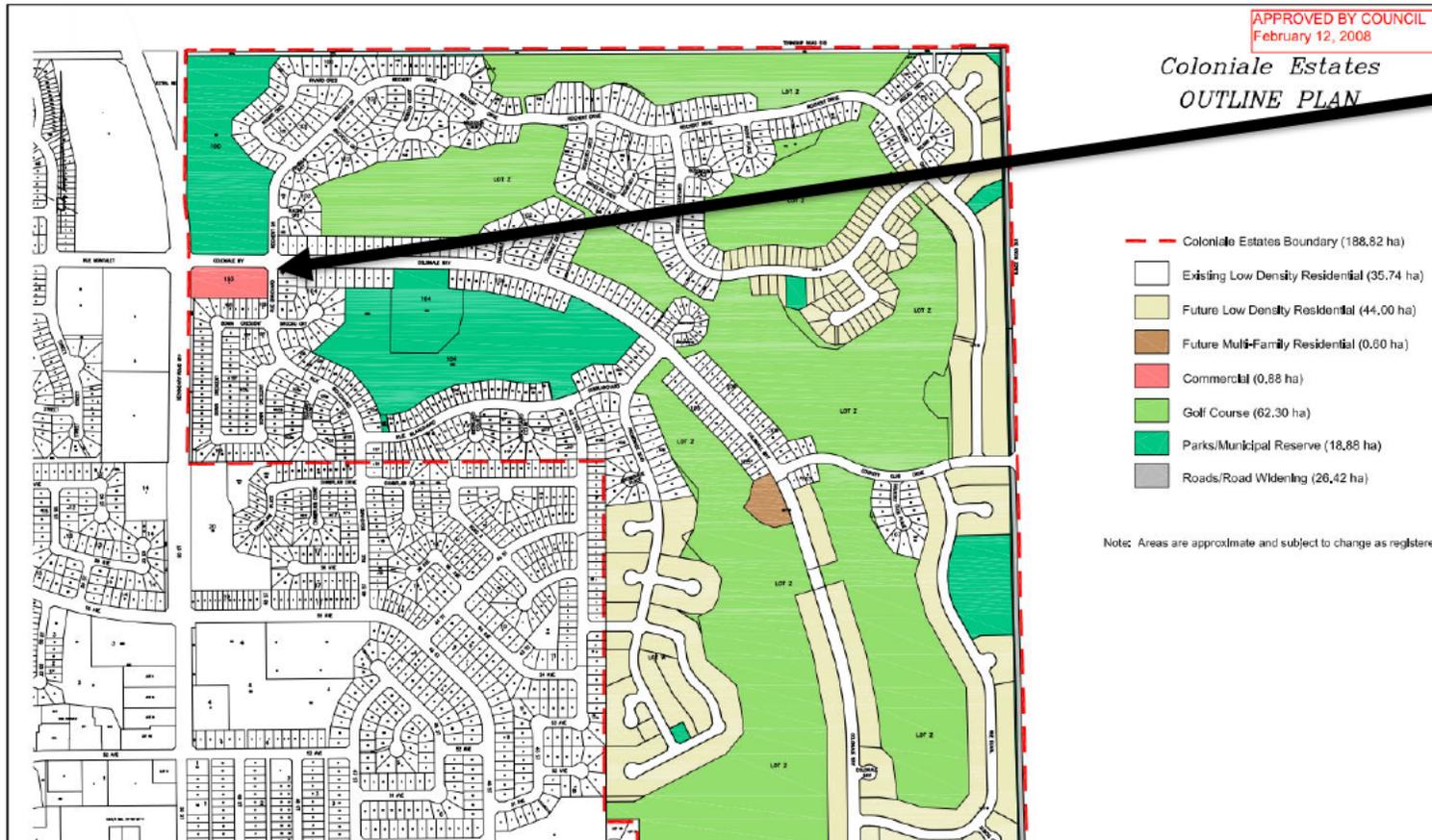
Section 4.13.5

This policy supports multi-storey commercial development as a means of using land and municipal infrastructure efficiently. The proposed form of development aligns with the policy objective of promoting more efficient land utilization.

Section 5.1.6

This policy encourages the provision of storefront commercial uses and a broad mix of commercial services. The proposed development contributes to this objective by supporting active commercial frontage and service diversity.

COLONIALE ESTATES OUTLINE PLAN



Identified as “Commercial”

TECHNICAL REVIEW

Development meets or exceeds requirements for:

- Setbacks
- Site Coverage
- Parking
- Landscaping

Orientation supports pedestrian activity along Rue Bouchard and Coloniale Way

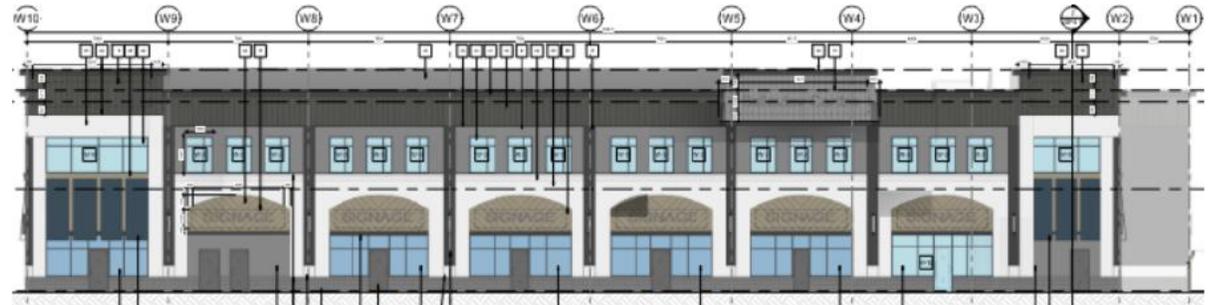
URBAN DESIGN & INTERFACE

Application was reviewed against the
Beaumont Urban Design Guidelines

Considered by the Design Review
Committee

City worked with applicant to enhance:

- Materials
- Articulation
- Glazing
- Treatment along Coloniale Way



PUBLIC NOTIFICATION

Section 5.16.1: The DA may require the applicant to post a notice on the property

- Describing the proposed
- Advising where information may be obtained
- Minimum 10 calendar days



PUBLIC NOTIFICATION

Signs were posted on the site from Nov. 7 to Dec 5th

City received several feedback submissions

Themes raised:

- Traffic & Safety
- Scale/Intensity
- Parking/spillover
- Neighbourhood Character

FEEDBACK CONSIDERATION

DA distinguished between:

- Matters within DP scope (compatibility, design, parking, interface)
- Matters outside DP scope (alternative land uses, broader policy, infrastructure)

Key issues (traffic, design, interface) were already addressed through technical review and design refinement.

What We Heard/What We Did report was shared with those who submitted feedback

DEVELOPMENT AUTHORITY'S CONCLUSION

- The “Built Form” complied with the Land Use Bylaw in all regards
- The Uses proposed are either Permitted or Discretionary
- The Development Authority was satisfied that the Discretionary Uses are reasonably compatible with surrounding uses. There was no solid planning reason to deny the applicant of their right to the two discretionary uses.
- The development aligns with the Municipal Development Plan and the Coloniale Estates Outline Plan.

QUESTIONS?



**NOTICE OF APPEAL
SUBDIVISION AND DEVELOPMENT APPEAL BOARD**

In accordance with Sections 678 and 686 of the *Municipal Government Act* and the City of Beaumont Bylaw, as amended, an appeal to the Subdivision and Development Appeal Board must be filed within the legislated timeframe and each Notice of Appeal must be accompanied by the legislated fee. For filling instructions and fee payment options, see reverse side of form.

| Development Permit (Check one box only) | | Subdivision Application (Check one box only) | | Notice of Order | |
|--|-------------------------------------|---|--------------------------|-----------------|--------------------------|
| Approval | <input checked="" type="checkbox"/> | Approval | <input type="checkbox"/> | Stop Order | <input type="checkbox"/> |
| Conditions of Approval | <input type="checkbox"/> | Conditions of Approval | <input type="checkbox"/> | | |
| Refusal | <input type="checkbox"/> | Refusal | <input type="checkbox"/> | | |

| SECTION 1 - MUNICIPAL ADDRESS OF SITE UNDER APPEAL | |
|--|-------------------------------|
| Municipal Address of Site Under Appeal | 9 Coloniale Way, Beaumont, AB |
| Application File # | 2025-077 |
| Date Appeal Application Received | |

| SECTION 2 - APPELLANT INFORMATION | |
|-----------------------------------|-----------------------|
| Appellant Name | Kevin Panter |
| Agent Name (if applicable) | |
| Address | [REDACTED] |
| Telephone | [REDACTED] Business # |
| Email (for notification purpose) | [REDACTED] |

Sections 678 and 686 of the *Municipal Government Act* requires that written Notice of Appeal must contain specific reasons for the appeal.

| SECTION 3 - REASONS FOR APPEAL |
|--|
| I do hereby appeal the decision of the Development Authority/Subdivision for the following reasons (attach separate page if required): <i>See attached (page 1-4)</i> |
| |
| |
| |
| |
| |

| SECTION 4 - PAYMENT (In accordance to Fees & Charges Bylaw, as amended) | | | | | | | |
|---|--------------------------|-------------|--------------------------|------|--------------------------|-------|-------------------------------------|
| Cheque | <input type="checkbox"/> | Money Order | <input type="checkbox"/> | Cash | <input type="checkbox"/> | Debit | <input checked="" type="checkbox"/> |

| SECTION 5 - SIGNATURE & DECLARATION | |
|--|-----------------|
| I certify that the information given on this form is true and complete to the best of my knowledge and acknowledge my authorization of the information to be used for the purposes identified below. | |
| SIGNATURE OF APPELLANT: [REDACTED] | DATE: 1/27/2026 |
| The personal information contained on this form is being collected under the authority of the City of Beaumont SDAB Bylaw and the <i>Municipal Government Act</i> (Sections 678 and 686) in accordance with the FOIP Act (Section 33(c)), for the purpose of handling development appeals. This information is protected by the privacy provisions of the FOIP Act. If you have questions about the collection, contact the FOIP Coordinator at the City of Beaumont, 5600 - 49 Street, Beaumont, Alberta T4X 1A1, or Phone 929-8782. NOTE: This information will form part of the file available to the public. | |

| FOR OFFICE USE ONLY | | | |
|--|--------------------|--------------|---------------------|
| Fee Paid Yes <input type="checkbox"/> No <input type="checkbox"/> | SDAB Appeal Number | Hearing Date | Date Received Stamp |

Section 3

Appeal Submission Points: 9 Colonial Way

1. Impact on Surrounding Residential Property Values and Residential Enjoyment

I am a property owner directly impacted by the approved development at 9 Colonial Way. My property backs onto the proposed site, and the approved design places a **main commercial road access directly behind my fence**, which will significantly affect both the **value of my property** and my **ability to reasonably enjoy it**.

The introduction of a two-storey commercial building and an active access roadway immediately adjacent to my rear yard will result in increased noise, vehicle headlights, traffic activity, and loss of privacy. These impacts are permanent in nature and materially diminish the residential character and quiet enjoyment of my property.

The **Municipal Development Plan (MDP) – Bylaw 938-19** emphasizes that development adjacent to residential uses must be **compatible in scale, intensity, and function** and must minimize adverse impacts on existing neighbourhoods. Placing a commercial access road directly behind an established residential lot does not meet the intent of these compatibility and livability objectives.

Further, the **Land Use Bylaw 944-19** is intended to protect residential properties from undue impacts caused by adjacent non-residential development, including noise, traffic, and visual intrusion. The location of a primary access route directly behind my fence fails to provide an appropriate transition or buffer between commercial and residential uses.

As both a nearby and directly affected property owner, I submit that this development approval does not adequately consider the **negative impacts on property value, residential enjoyment, and livability**, and therefore does not align with the intent of Beaumont's statutory planning policies.

2. Incompatibility with Municipal Development Plan (MDP) Policies

a) Commercial Development Must Be Compatible with Adjacent Residential

Beaumont's **Municipal Development Plan (MDP) — Our Complete Community** requires land uses adjacent to residential areas to be planned so they are **compatible with surrounding development** and respect neighbourhood character.

Commercial buildings immediately adjacent to low-density residential areas (especially two-storey structures) should be located and designed to avoid adverse impacts such as shading, noise, traffic intrusion, and impacts on residential amenity; all of which influence

property values and neighbourhood character. The current permit approval does not demonstrate how the scale and height of the commercial building on a residential block adequately meets these compatibility objectives.

3. Land Use Bylaw (LUB) 944-19 — Parking & Building Form

a) Parking Requirements

Beaumont's **Land Use Bylaw 944-19** governs parking regulations and design standards for commercial developments. The LUB includes specific **parking requirements** based on land use type, intended to ensure adequate on-site parking is available for customers, staff, and visitors without creating spillover onto residential streets.

- The development's reliance on **on-street parking along Coloniale Way** for at least five spaces results in insufficient on-site parking capacity, especially during peak uses (sporting events, school pickup/drop-off). This contradicts the intent of the LUB's structured parking standards.
- Insufficient parking can lead to spill-over onto residential streets, safety concerns, and obstruction of traffic; outcomes the LUB parking standards are designed to avoid.

Appeal point:

The Development Authority's approval fails to demonstrate compliance with the parking requirements and intent of the Land Use Bylaw 944-19 regarding adequate parking supply for commercial uses. The reliance on on-street parking for commercial users creates spillover parking demands inconsistent with LUB objectives to protect residential streets.

b) Building Height & Compatibility

The LUB regulates building heights and massing to ensure new development fits the character of the area in which it is located. A two-storey commercial building adjacent to low-density residential properties requires careful consideration of "fit and function," site design, and impact on surrounding properties.

Appeal point:

The Development Authority did not provide adequate justification that the scale and height of the proposed building meets the LUB's intent to maintain compatibility with the surrounding residential context.

4. Traffic, Access & Safety Concerns

a) Traffic Impacts

While the **Transportation Master Plan** for Beaumont (referenced in statutory planning documents) encourages efficient integration of transportation infrastructure, it also anticipates that traffic impact assessments and road design should ensure safe access for all users.

- The proposed development adds exit/entry points on Coloniale Way near existing intersections and a traffic light, creating conflict points and compounding existing congestion before/after school and community events.
- This affects traffic flow, safety, and emergency vehicle access, this is especially problematic on an existing residential street.

Appeal point:

No un-biased traffic impact analysis appears to have been provided to justify the safety of the access/egress configuration, which undermines the objectives of Beaumont's transportation planning framework.

b) Pedestrian Safety at Crosswalks

The intersection of **Rue Bouchard and Colonial Way** has recently required upgrades (e.g., enhanced crosswalks and lights) due to documented safety concerns for schoolchildren and pedestrians in the neighbourhood.

Appeal point:

Additional commercial traffic generated by this development will increase pedestrian/vehicle conflicts at this established crossing location unless a thorough safety study and mitigation measures are demonstrated.

5. Infrastructure Wear & Community Impact

Beaumont's planning regime anticipates that developments must not impose unreasonable burdens on residential infrastructure. The presence of a commercial site generating frequent delivery vehicle traffic, in addition to construction vehicles during building phases, will accelerate wear and tear on roads designed for residential use.

Appeal point:

The Development Authority's decision did not demonstrate assessment of the impact of added heavy vehicle traffic on residential roads, which is inconsistent with the intention of LUB planning for appropriate land use locations.

6. Strategic Land Use Location & Strip-Mall Saturation

Beaumont's MDP encourages a more strategic placement of commercial activities within designated commercial nodes and employment centres (as seen in regional planning

frameworks). Rail-side, collector, or arterial frontage locations are traditionally preferred for commercial uses rather than placing small commercial sites within residential networks.

Appeal point:

The approval of this strip-mall style commercial building conflicts with broader strategic planning goals of locating commercial uses where they support complete communities without undermining residential character.

Conclusion

As a directly affected property owner whose residential property backs onto the proposed development at 9 Colonial Way, I will experience significant and permanent impacts as a result of this approval. The placement of a two-storey commercial building and a **main commercial access roadway directly behind my fence** will negatively affect both the **value of my property** and my **reasonable enjoyment of it**, through increased noise, traffic, loss of privacy, and visual intrusion.

The approved development does not adequately address compatibility with the surrounding residential neighbourhood and fails to align with the intent of the **Municipal Development Plan (Bylaw 938-19)** and the **Land Use Bylaw (Bylaw 944-19)**, which are intended to protect residential amenity, ensure safe and efficient transportation, and promote orderly and appropriate land use.

Further, the decision does not sufficiently consider the cumulative impacts of traffic congestion, pedestrian safety (particularly at the Rue Bouchard and Colonial Way crosswalk) parking shortfalls, infrastructure strain on residential roads, and the inappropriate scale and placement of commercial development within a residential area.

For these reasons, and due to the direct and material impacts on my property and neighbourhood, I respectfully submit that the development permit approval for 9 Coloniale Way is not consistent with sound planning principles or Beaumont's statutory policies and should be reconsidered or overturned by the Subdivision and Development Appeal Board.

Thank you,

Kevin Panter



February 17, 2026

Appeal No. SDAB-26-01

9 Coloniale Way, Beaumont, Alberta

Development Permit Application No. 2025-077

Land Use District: Integrated Neighbourhood (IN)

Proposed Development:

Two 2-storey Commercial (CRU) Buildings – East Building

includes Daycare on the upper floor with retail, service, general and restaurant/café uses below.

Legal Description: Plan 912 1656, Block 103, Lot 1

Submitted by:

Green Space Alliance (Planning Consultant to the Applicant)

RE: Submission to the Subdivision and Development Appeal Board

Dear SDAB Members:

1. Introduction

This submission supports the Development Authority's decision to approve the Development Permit for two commercial buildings at 9 Coloniale Way, including a daycare (upper floor of the east building) and neighbourhood-serving commercial uses at grade.

The appeal raises concerns regarding compatibility, traffic and safety, parking, infrastructure impact, and strategic land use direction under Beaumont's Municipal Development Plan (MDP) – Bylaw 938-19.

Pursuant to section 687(3)(d) of the Municipal Government Act, the Board must decide the appeal in accordance with the Land Use Bylaw and any applicable statutory plans in force at the time of its decision. The Board's task is therefore to assess whether the approved development complies with and reasonably implements those instruments.

2. Statutory Policy Context – Municipal Development Plan (Bylaw 938-19)

2.1. Land Use Designation – Map 9

Map 9 of the MDP designates the subject site as **Commercial**. The MDP provides that commercial uses are encouraged in the locations identified on Map 9.

Where a statutory plan designates land for a particular use, that designation provides the policy framework for subsequent zoning and development decisions.

The question before the Board is therefore not whether commercial use is appropriate in principle, but whether the approved development appropriately implements that designation.

2.2. Complete Community Objectives

The MDP supports complete communities through the integration of neighbourhood services and amenities within residential areas. Relevant policies include:

- Provision of neighbourhood-serving commercial uses;
- Reduction of unnecessary vehicle trips;
- Integration of services within walking distance of homes.

This proposal includes neighbourhood-scale commercial uses and a daycare facility serving nearby residents. It advances the MDP's strategic objectives rather than undermining them.

2.3. Compatibility and Interface Policies

Policy 4.8.3 provides that development should be sensitive to the scale and design of the surrounding built environment.

Policy 4.8.8(a) requires consideration of compatibility of height and massing with adjacent land uses.

Policy 4.8.5 identifies daycare facilities as neighbourhood services that should be considered in established residential areas.

Compatibility does not require preservation of existing conditions. The Municipal Development Plan anticipates change over time, and the Land Use Bylaw establishes the built-form and interface standards through which that change is managed. Compatibility is therefore assessed through compliance with height, setback, massing, landscaping, and buffering regulations. The approved development satisfies those standards and implements the MDP's compatibility policies through site design.

3. Land Use Bylaw Compliance – Integrated Neighbourhood District

The subject site is zoned Integrated Neighbourhood (IN). The Development Permit:

- Requires no variances;
- Meets height limits;
- Meets setback and separation requirements;
- Meets parking regulations;
- Includes discretionary uses contemplated within the district.

The Land Use Bylaw is the instrument through which Council implements MDP policy. The Board must therefore assess this proposal against the IN-District regulations as written.

4. Site Context and Interface

The subject site is a corner lot with the following interfaces:

- South: single-detached residential parcels
- East: residential across the street
- North: public park and Coloniale Way (collector road)
- West: 50 Street (major arterial), with more intense commercial uses west of the arterial

This is a transitional edge condition between neighbourhood and arterial contexts.

4.1. Access Design

Access is configured as follows:

- Primary all-directional access from Coloniale Way (collector);
- All-directional exit to Rue Bouchard (collector);
- Secondary right-in-only access from 50 Street (arterial), designed as a safety measure.

No access relies on local residential streets.

Concerns regarding traffic operations and pedestrian safety are addressed through a Transportation Impact Assessment prepared by a qualified professional. The consultant will speak about peak hour analysis and pedestrian conflict.

4.2. Building Massing and Transition

The daycare is located on the second floor of the east building. This building is designed as a two-storey form at the north end of the site, stepping down in height toward the south and Rue Bouchard. At its closest interface with the residential neighbours, the building transitions to a single storey, reducing perceived mass and providing a more appropriate scale along the residential edge.

The west building is a full two-storey structure situated at the northwest corner of the site. It is oriented toward the arterial context, addressing the primary intersection of Coloniale Way and 50th Street, as well as the park to the north.

Between the buildings and the southern residential parcels, the following elements provide transition and buffering:

- Surface parking areas screened with planting;
- Internal circulation routes, including vehicular access and 2-metre-wide pedestrian pathways;
- Landscape buffering and fencing, with additional trees planted between the residential properties and internal roadways. While some existing trees will need to be removed or relocated, the completed development will result in a net increase in trees on site;
- Molok waste management systems to maintain a tidy appearance, reduce the risk of odours, and improve overall site aesthetics.

Locating surface parking toward the interior of the site is a requirement of the zoning by-law. Alternative building orientations were explored but ultimately dismissed, as they were less appropriate and did not uphold the intent of the zoning framework.

The buildings will be constructed using high-quality, durable materials. A masonry base anchors each building, with EIFS, wood-look metal panels, and metal cladding above. The architectural expression is a contemporary interpretation of French colonial design, subtly referencing a mansard roof form at the second floor. Arched storefront-style entries soften the façade, complemented by quaint projecting signage and generous transparent glazing.

High-quality light fixtures with soft, downward-directed illumination are proposed around the buildings. These fixtures have been submitted for review and deemed appropriate, meeting the requirements of the development.

All rooftop mechanical equipment will be screened through integrated architectural features and additional screening where necessary to minimize visual impact. This layered separation satisfies the compatibility considerations in Policy 4.8.8(a).

5. Parking

The Development Permit complies with all parking requirements of Bylaw 944-19.

The on-street parking along Coloniale Way pre-existed the application and is not relied upon to meet minimum requirements.

Additionally, even with the required Daycare loading/drop-off designated spaces, the remaining parking still meets the required parking requirements.

Due to this development's proximity to the park and residential zones, additional bicycle parking has been proposed to encourage bicycle traffic.

Concerns regarding potential spillover parking are speculative and do not reflect non-compliance with the applicable parking standards.

6. Traffic and School Operations

The Transportation Impact Assessment concludes:

- Peak hours of the development do not overlap with school peak periods;
- Daycare drop-off and pick-up periods do not conflict with school operations;
- The increase in traffic will not materially increase pedestrian-vehicle conflict.

The transportation consultant will address these findings directly at the hearing.

7. Infrastructure Wear and Community Impact

The appeal suggests that commercial traffic will impose unreasonable burdens on residential infrastructure.

Access is provided via collector and arterial roads.

Infrastructure wear and lifecycle management are municipal engineering functions addressed through road standards and capital planning. They are not determinative land use tests under the MGA or Land Use Bylaw.

Absent evidence of unsafe access or bylaw non-compliance, infrastructure wear concerns do not provide a basis for overturning an otherwise compliant permit.

8. Strategic Land Use Location

The appeal asserts that commercial uses should be located elsewhere.

However:

- The MDP designates the site Commercial (Map 9);
- The IN district implements that designation;
- The development provides neighbourhood-serving uses consistent with complete community objectives.

Strategic land use direction is established through the Municipal Development Plan and implemented through zoning. The Board's role is to apply the adopted framework currently in force. Its assessment is therefore grounded in compliance with the Land Use Bylaw and reasonable exercise of discretion within that statutory structure.

9. Conclusion

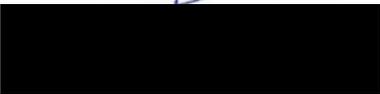
The Development Permit:

- Implements the MDP's Commercial designation (Map 9);
- Advances complete community objectives;
- Complies with the Integrated Neighbourhood district regulations;
- Requires no variances;
- Provides appropriate buffering and transition;
- Is supported by professional transportation analysis.

While change can raise understandable concerns, the issue before the Board is whether the approved development complies with and reasonably implements the statutory planning framework in force. On that basis, the Development Permit represents a regulation-compliant and policy-consistent implementation of Beaumont's adopted planning framework.

For these reasons, we respectfully request that the Board confirm the Development Authority's decision.

Respectfully submitted,



Marcelo Figueira, MEdes, RPP, MCIP

Associate Principal

GSA Consulting Inc. (Green Space Alliance)

Cc: Dnyanesh Deshpande, Principal, GSA Consulting Inc.